



AGENDA

ASTORIA CITY COUNCIL

November 16, 2015

7:00 p.m.

2nd Floor Council Chambers

1095 Duane Street · Astoria OR 97103

1. CALL TO ORDER

2. ROLL CALL

3. REPORTS OF COUNCILORS

4. CHANGES TO AGENDA

5. CONSENT CALENDAR

The items on the Consent Calendar are considered routine and will be adopted by one motion unless a member of the City Council requests to have any item considered separately. Members of the community may have an item removed if they contact the City Manager by 5:00 p.m. the day of the meeting.

- (a) City Council Minutes of 10-19-15
- (b) City Council Work Session Minutes of 10-19-15
- (c) Boards and Commissions Minutes
 - (1) Historic Landmarks Commission Meeting of 9-1-15
 - (2) Planning Commission Meeting of 8-25-15
- (d) Waiver of Downtown Overtime Parking During the Holiday Season

6. REGULAR AGENDA ITEMS

All agenda items are open for public comment following deliberation by the City Council. Rather than asking for public comment after each agenda item, the Mayor asks that audience members raise their hands if they want to speak to the item and they will be recognized. In order to respect everyone's time, comments will be limited to 3 minutes.

- (a) Public Hearing and Ordinance – A15-03 Ordinance regarding Development of Code and Comprehensive Plan Language and Corresponding Map Amendments to Implement the Neighborhood Greenway (41st to 54th) Area of the Riverfront Vision Plan (1st reading) (Community Development)
- (b) Appeals by Ron Zilli for the Wireless Communication Facility Permits at 1580 Shively Park Road (*Request to Continue Hearings to the December 21, 2015 City Council Meeting*) (Community Development)
 - (1) Appeal (AP15-01) On New Construction Permit (NC15-03)
 - (2) Appeal (AP15-02) on Variance (V15-03)
 - (3) Appeal (AP15-03) on Wireless Communication Facility (WCF15-03)
- (c) Right Turn Permitted Without Stopping Investigation (Public Works)
- (d) Temporary Public Restrooms (Police)

7. NEW BUSINESS & MISCELLANEOUS, PUBLIC COMMENTS (NON-AGENDA)

THIS MEETING IS ACCESSIBLE TO THE DISABLED. AN INTERPRETER FOR THE HEARING IMPAIRED MAY BE REQUESTED UNDER THE TERMS OF ORS 192.630 BY CONTACTING JULIE YUILL, CITY MANAGER'S OFFICE, 503-325-5824.



CITY OF ASTORIA
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November 13, 2015

M E M O R A N D U M

TO: MAYOR AND CITY COUNCIL

FROM:  BRETT ESTES, CITY MANAGER

SUBJECT: ASTORIA CITY COUNCIL MEETING OF NOVEMBER 16, 2015

CONSENT CALENDAR

Item 5(a): City Council Minutes

The minutes of the City Council meeting of October 19, 2015 are enclosed for review. Unless there are any corrections, it is recommended that Council approve these minutes.

Item 5(b): City Council Work Session Minutes

The minutes of the City Council work session of October 19, 2015 are enclosed for review. Unless there are any corrections, it is recommended that Council approve these minutes.

Item 5(c): Boards and Commissions Minutes

The minutes of the (1) Historic Landmarks Commission meeting of September 1, 2015, and (2) Planning Commission meeting of August 25, 2015 are enclosed. Unless there are any questions or comments regarding the contents of these minutes, they are presented for information only.

Item 5(d): Waiver of Downtown Overtime Parking During the Holiday Season

Alana Garner, Executive Director for the Astoria Downtown Historic District Association (ADHDA), is requesting that the City Council implement a waiver of overtime parking for the upcoming holiday season from Thanksgiving Day, November 26, 2015 through Sunday, January 3, 2015. The intention is for no enforcement of overtime parking in the downtown, which includes the Heritage Square parking lot, as well as on-street parking; however, tickets will still be written for other violations in the Parking District, including parking by downtown employees within the District. The City of Astoria has approved this request of ADHDA for several consecutive years; therefore, it is recommended that Council approve this request.

REGULAR AGENDA ITEMS

Item 6(a): Public Hearing and Ordinance – A15-03 Ordinance regarding Development of Code and Comprehensive Plan Language and Corresponding Map Amendments to Implement the Neighborhood Greenway (41st to 54th) Area of the Riverfront Vision Plan (1st reading) (Community Development)

In 2008-2009, the City of Astoria developed the Riverfront Vision Plan (RVP) to address issues dealing with open space, land use, and transportation issues along the Columbia River. Significant public involvement opportunities were designed to gain public input. This process was initiated to plan for these issues in a comprehensive manner and to set a framework for the future of the study area. The City's north Riverfront (Columbia River to West Marine / Marine Drive / Lief Erikson Drive) was divided into four Plan areas of development: Bridge Vista (Port/Smith Point to 2nd Street), Urban Core (2nd to 16th Street), Civic Greenway (16th to 41st Street), and Neighborhood Greenway (41st Street to 54th Street, east end of Alderbrook Lagoon). City Council accepted the Riverfront Vision Plan in December 2009. Since that time, the City Council has set goals regarding implementation of the Riverfront Vision Plan. Implementation of recommendations from the Riverfront Vision Plan in the Neighborhood Greenway Plan Area will take the form of map amendments, Development Code and Comprehensive Plan amendments.

Proposed map amendments will include: 1) Apply the new Neighborhood Greenway Overlay (CGO) Zone to the Neighborhood Greenway Plan Area; and 2) Rezone the water area between 41st and 54th Streets between the shoreline to the pier head line from A-3 (Aquatic Conservation) to A-4 (Aquatic Natural).

Proposed Development Code text amendments will include: 1) Add Neighborhood Greenway Overlay Zone to address the standards for over-water development including structure height and width, allowable uses, and landscaping; 2) Add new design standards for multi-family development in the Neighborhood Greenway Plan Area; 3) Add new design guidelines for multi-family residential and non-residential development in the Neighborhood Greenway Plan Area; 4) Establish landscaping standards for multi-family residential and non-residential construction/uses. There would be no landscaping standards for single-family and two-family dwellings; 5) Allow some exemptions for the few existing over-water buildings to continue to be viable businesses thereby preserving the historic structures; 6) Limit new, over-water development to maximum height of top of bank; and 7) Make miscellaneous "housekeeping" amendments related to references to the above noted amendments.

Proposed Comprehensive Plan text amendments will include: 1) Update the description of the Alderbrook Area and reference the Neighborhood Greenway Overlay Area and Riverfront Vision Plan implementation; 2) Acknowledge the growing impact of traffic to the neighborhood; 3) Add a policy to investigate the possibility of extending the trolley to the Alderbrook area; and 4) Change designation of aquatic area from conservation to natural, and amend allowable

uses in the Aquatic Natural designated areas to include the exception for existing structures.

The Planning Commission held a public hearing at the October 27, 2015 APC meeting and unanimously recommended that the Council adopt the proposed amendments. A public hearing on the Amendment is proposed for the November 16, 2015 City Council meeting. It is recommended that Council hold a public hearing and consider adoption of the proposed ordinances. If the Council is in agreement with the recommendation of the Planning Commission, it would be in order for Council to hold a first reading of the two separate ordinances.

Item 6(b): Appeals by Ron Zilli for the Wireless Communication Facility Permits at 1580 Shively Park Road (Request to Continue Hearings to the December 21, 2015 City Council Meeting) (Community Development)

(1) Appeal (AP15-01) On New Construction Permit (NC15-03)

On August 3, 2015, Verizon Wireless LLC applied for a New Construction permit (NC15-03) to the Historic Landmarks Commission (HLC) to construct a new wireless communication facility at 1580 Shively Park Road within Shively Park. On September 15, 2015, the HLC held a public hearing and approved the request with conditions. A Notice of Appeal on the HLC decision was submitted by Ron Zilli on September 30, 2015 stating that the request should be denied listing several issues for the basis of denial. A public hearing on the Appeal has been advertised and is scheduled for the November 16, 2015 City Council meeting. The applicant has requested that the hearing be continued to the December 21, 2015 City Council meeting. It is recommended that the City Council continue the public hearing to December 21, 2015.

(2) Appeal (AP15-02) on Variance (V15-03)

On August 3, 2015, Verizon Wireless LLC applied for a Variance permit (V15-03) to the Astoria Planning Commission (HLC) to construct a new wireless communication facility at 1580 Shively Park Road within Shively Park with a height of 150' which would exceed the maximum 45' height. On September 16, 2015, the APC held a public hearing and approved the request with conditions. A Notice of Appeal on the APC decision was submitted by Ron Zilli on September 30, 2015 stating that the request should be denied listing several issues for the basis of denial. A public hearing on the Appeal has been advertised and is scheduled for the November 16, 2015 City Council meeting. The applicant has requested that the hearing be continued to the December 21, 2015 City Council meeting. It is recommended that the City Council continue the public hearing to December 21, 2015.

(3) Appeal (AP15-03) on Wireless Communication Facility (WCF15-03)

On August 3, 2015, Verizon Wireless LLC applied for a Wireless Communications Facility permit (WCF15-03) to the Astoria Planning Commission (HLC) to construct a new wireless communication facility at

1580 Shively Park Road within Shively Park. On September 16, 2015, the APC held a public hearing and approved the request with conditions. A Notice of Appeal on the APC decision was submitted by Ron Zilli on September 30, 2015 stating that the request should be denied listing several issues for the basis of denial. A public hearing on the Appeal has been advertised and is scheduled for the November 16, 2015 City Council meeting. The applicant has requested that the hearing be continued to the December 21, 2015 City Council meeting. It is recommended that the City Council continue the public hearing to December 21, 2015.

Item 6(c): Right Turn Permitted Without Stopping Investigation (Public Works)

There are several primary routes through the City that allow “Right Turn Permitted Without Stopping” secondary signs below stop signs to facilitate movement. While this existing condition has proved successful for many years, it does have some drawbacks that have resulted in complaints. DKS Associates, a transportation engineering and planning firm, investigated the situation. They recently completed both the City of Astoria and Clatsop County Transportation System Plans. DKS has reviewed relevant travel paths through town to identify and will provide recommendations at the City Council meeting. DKS presented the results of their study to the Traffic Safety Advisory Committee at their October 27th meeting. The Traffic Safety Committee recommends adoption of Option 4. Examples include the possible placement of a dedicated right turn lane at the foot of 7th Street turning northbound onto Highway 202 and “Stop Sign Ahead” signs where stop sign violations are known to occur at a rate higher than expected. It is estimated that the cost for these changes will be around \$10,000. This issue is being forwarded to Council to determine if funds should be allocated to address the concerns. Should Council concur with Option 4, Public Works staff will work to implement this within the current Fiscal Year budget.

Item 6(d): Temporary Public Restrooms (Police)

Over the previous summer, the number of complaints the Astoria Police Department has responded to regarding public urination and defecation is greatly increased. We have also heard from business owners in the downtown that this is a major issue affecting them regularly. The clear message received from the Astoria Downtown Historic District Association was that this is not just an issue with the members of our community who are homeless but also for tourists who are visiting the community. While public restrooms are available at the Sunset Empire Transportation District office and the City owned restrooms are available east of 12th on Exchange these facilities are not located where they positively affect this issue.

Since September, the City of Astoria Coalition on Community Homeless Interaction has been meeting. While they are moving toward some other recommendations, one recommendation that was quite unified and was deemed as “low hanging fruit” is to site more public restrooms. This recommendation was unanimous.

Overlaying this issue is the ongoing problems Astoria Parks and Recreation faces with vandalism to existing bathrooms. There has been significant repeated damage to the downtown bathrooms, the Doughboy Monument bathrooms and the bathrooms at Tapiola Park.

Long term, staff has identified a potential permanent solution to the issue of public restrooms with a facility referred to as the Portland Loo. While these units have a fairly high initial purchase price they are designed in a way that incorporates the concepts of crime prevention through environmental design (CEPTED), a proven community policing and planning concept for crime reduction. These solutions would be brought back to Council as part of the budget process for FY 16-17 if they develop.

In the interim, Angela Cosby and Brad Johnston have developed two locations where temporary portable toilet facilities could be located. The criteria for locations were that they had to be:

- High traffic areas that offered easy visual surveillance (the presence of many eyes reduces crime).
- Not over or under combustible materials (to avoid associated fires where portable are vandalized with fire).
- In an area where they can be easily serviced.
- In a location where their presence would not be visually shocking.
- In an area where calls, complaints, and anecdotal evidence demonstrate a need for public restrooms.

Given these criteria the Parks Director, Angela Cosby, and Police Chief, Brad Johnston, recommend that the City contract to place two portable toilets at People's Park (16th and Marine) in the parking area and one portable toilet at 9th and Astor in the right of way near the street terminus where concrete blocks currently block the street. These toilets would be serviced twice a week. Funding for the service and toilets would come from the Promote Astoria Fund as these are services provided to support tourism. Cost for this service is estimated to be \$130.40 per unit per 28 day billing period. Staff recommends purchasing the \$7.95 per billing period damage waiver. If delivery is scheduled to coincide with routine trips, there will be no charge for delivery or set up. The total cost for one year of service is \$5,395.65.

Staff will continue to monitor the service levels and explore the possibility of more permanent solutions that may alleviate some of the issues which cause ongoing problems for Astoria Parks and Recreation with the existing public restrooms. It is recommended that Council contract for placement of three temporary toilets.

A regular meeting of the Astoria Common Council was held at the above place at the hour of 7:05 pm.

Councilors Present: Nemlowill, Herzig, Warr, Price, Mayor LaMear

Councilors Excused: None

Staff Present: City Manager Estes, Assistant City Manager/Police Chief Johnston, Community Development Director Cronin, Parks and Recreation Director Cosby, Finance Director Brooks, Fire Chief Ames, Library Director Tucker, Public Works Director Cook, and City Attorney Henningsgaard. The meeting is recorded and will be transcribed by ABC Transcription Services, Inc.

REPORTS OF COUNCILORS

Item 1(a): Councilor Nemlowill had no report.

Item 1(b): Councilor Herzig reported the Lower Columbia Diversity Project (LCDP) held a presentation on community policing on Thursday, October 15. Police chiefs from Astoria, Seaside, Warrenton, and Cannon Beach attended the event, which included a great community conversation. The next LCDP presentation will be on Sunday, October 25 at 1:00 pm at the Boynton Building. Tony Johnson, Tribal Chairman of the Chinook Nation will give a presentation on the Nation's petition to the federal government to reaffirm its recognition of the Tribe. The LCDP was contacted by the Tongue Point Job Corps requesting diversity training for Tongue Point staff. The training will be conducted later in October. His next meet the public event was scheduled for Saturday, October 24 at 12:30 pm in the Flag Room at the Astoria Library. He hosts these events every month and they are open to the public. He believed Staff needed more comfortable chairs to sit in during Council meetings and asked that Council direct the City Manager to look into purchasing comfortable chairs for Staff.

Item 1(c): Councilor Price reported that she attended a small concert at 46 North Farm in Olney with Luke Ydstie, members of Blind Pilot, and an Australian singer/songwriter. The sound was excellent and the sound engineer at the concert works for KMUN and other organizations. This event was a clear and shining example of a new economy that is a mix of music, art of all types, food webs, and a resurgence of agriculture. This parallels Astoria's traditional businesses and heritage of logging and fishing. She also went on a forestry tour through two active logging operations on October 8. She learned that the logging industry still offers well-paying jobs, but young people are not interested in taking those jobs. She also learned that Hampton Saw Mill in Warrenton only runs at between 65 and 75 percent capacity because they are unable to obtain enough raw materials to run the mill at full capacity. The mill believes this is because logging in Clatsop State Forest is restricted to about 60 percent of annual harvest. Private lands can be harvested differently, where the mill can take about 100 percent of the annual growth of a forest. She wanted people to know that there is a lot of pressure in the local community and other communities around the state that heavily depend on logging. Many businesses and legislatures advocate for larger harvests. There are two very different economies running side by side, both of which are quite vibrant in Astoria.

Item 1(d): Councilor Warr had nothing to report.

Item 1(e): Mayor LaMear recognized Astoria Port Commissioner Steve Fulton and all City Board and Commission members. She reported that she also attended the forestry tour. She looks forward to the tour every year, which visits a variety of forests owned by various entities. It is fun to hear different perspectives about the timber industry. She also participated in the Great Oregon Shake Out at City Hall, which revealed some glitches that are now being addressed. A group of people was left sitting in Council Chambers because Staff believed the loud speaker could be heard from inside the room. Everyone else was out in the parking lot when Staff realized a group of people was still in the meeting room. This is why drills are necessary. She and 30 other mayors from other coastal cities in the United States will attend the Rising Tides 2015 Conference on October 21 – 25 in New Hampshire to discuss rising sea levels. Pat Corcoran with the Oregon State University Extension office has told her that the subduction zone that begins at Mendocino, CA and extends north is not experiencing rising sea

levels because the land is rising at the same rate as the sea. However, when a large earthquake occurs, the sea level will rise all at once and create a tsunami. While the conference will focus on rising sea levels, ocean acidification is also harming shellfish and the seafood industry. She is looking forward to hearing the different perspectives from around the United States. She announced that she has appointed Jim Holen and Ken Hageman to the Parks and Recreation Master Plan Committee. She wanted to appoint someone involved with youth in the community and Mr. Haagman is President of the Little League. Astoria has many different committees because the City is working on a lot of different issues.

PRESENTATIONS

Item 2(a): Rich Mays, Clatsop County Interim Manager

Interim Clatsop County Manager Rich Mays introduced himself and noted his past experience as a city manager in several cities. Upon retiring, he and his wife moved to the area. A few months ago, he was approached by several members of the County Board about serving as interim manager because he is familiar with the area. He accepted the position and has been on the job for three weeks. His most important task is to hire a county manager. The County Board will hold a work session on the vacant county manager position on Wednesday, October 21 at the Boynton Building. The County has hired a consultant to assist in recruiting a new county manager through a nationwide search. The County will also host a senior officials' work shop on tsunami and earthquake preparedness. The work shop is intended to share strategies, coordinate plans for emergency preparedness and response, and to discuss executive level issues with leaders from the agencies involved in all levels of the emergency management structure, which includes all elected officials and Staff. He handed out a flyer about the event to City Council and Staff. When he worked for Cannon Beach, he enjoyed the relationship with Astoria and he looked forward to spending some time over the next several months working with City Manager Estes and City Council. He confirmed the public was welcome to attend the work session on hiring a new county manager.

Item 2(b): Bear Creek Dam Seismic Study Status Report

Cornforth Consultants has completed a preliminary seismic stability evaluation of the Bear Creek Dam and will be presenting an overview of their results at the Council meeting. They will also be available to answer any questions regarding the study and the scope of their services for additional geotechnical investigation work needed to complete their analysis.

Engineer Harrington explained that Staff has been studying the seismic vulnerabilities of the Bear Creek Dam for a couple of years. The last study, done in 1993, indicated the dam needed some repair work, which was so cost prohibitive at the time that the work was not completed. The dam is located about 12 miles southeast of Astoria, just outside of Swenson, at the very base of the watershed. The results of the analysis were very good for one side of the dam, but it is very important to learn more about the questionable side of the dam to assess the overall structural strength of the entire structure. The dam has been classified as high hazard dam by the State of Oregon Water Resource Department. This means that failure of the dam will result in property damage and potential loss of life downstream. The State is so interested in making sure the necessary repair work is completed that they have offered a grant to pay for half of the costs. The results of this study will provide valuable information that can be used for other projects around the state.

Councilor Nemlowill asked what kinds of property and which people could be affected if the dam fails. Engineer Harrington said about 130 to 170 homes are located downstream of the dam within the inundation zone. The study completed in 1993 assumed the dam would break in half, flooding Bear Creek with about 200 million gallons of water. It is not likely the dam will fail in this way. If it fails, the dam could move, leak, or crack, spilling water from several places. This study will confirm the most likely scenario. If the study leads to an unacceptable inundation zone, Staff will consider ways to mitigate and prevent failure. The main objective is to prevent any downstream damage. The Cascadia subduction zone is a phenomenon that is still being studied; it was only a theory when the 1993 study was being done. Since then, earthquakes have provided valuable information and there are new modeling techniques that get much closer to the truth. The dam was built by Astoria Water Works in 1912, and raised for more capacity in 1953. Astoria Water Works was a private company that sold water to the citizens of Astoria. The City purchased the dam in the 1930s. Astoria also owns the watershed, which allows the City to control harvest practices and spraying.

Councilor Price understood the desire and need to mitigate for damage and the inundation of 129 properties and 69 homes. She asked if the study was required and if so, by whom. She also wanted to know the consequences of not completing the study and what kind of event the dam is expected to survive. Engineer Harrington explained that Astoria is under the State's jurisdiction and the State requires the City to regulate its dams. Astoria has two earthen dams and one concrete dam, which is the Bear Creek Dam. The State inspects the dams annually and gives the City a list of required repairs. If the repairs are not made, fines are assessed. Astoria has a cooperative relationship with the State, which is offering to help the City complete the project. No restrictive timeframes have been imposed by the State. The State has a strong desire to move this project forward and losing momentum at this time would not be helpful. Grant funds paid for a portion of the first phase, and a grant has been offered for the next phase. To date, the State has paid for about 40 percent of the project. Assumptions have been made about how the dam will react, but there is no way to know how the dam will perform until an event occurs. However, the assumptions get as close as possible to what could happen. Staff anticipates the next phase of the study will indicate that dam failure is unlikely or will be minimal.

Gerry Heslin, Cornforth Consultants, gave a PowerPoint presentation on the technical analysis of Bear Creek Dam. He reviewed the project objectives, described the geologic model created for the study, explained analysis methods, results and conclusions, and recommended next steps. The model used in the last study did not support abutments and therefore indicated the dam should have failed decades ago. The new model included support abutments and considered information contained in Astoria's City Engineer Field Notes from 1912. This model is designed to calculate the sliding stability of the dam and estimate how much the dam will move during an earthquake. He gave a detailed description of how the dam was built, based on information contained in the Field Notes. This information was used to create the new geologic model. One-third of the dam was built on good basalt rock, the middle is on weak sandstone, and the right abutment is on an unknown substrate. The study will concentrate on right abutment. Once the model analyzes the stability of sections of the dam, the data will be used to determine the stability of the entire structure. He explained the technical details about how the model will calculate stability and described the complex geology of the area, noting that the dam is located in the best spot within 50 miles. However, the dam is wedged in between basalt rock and floats in a sea of much weaker marine sediments. This geology changes drastically and quickly. The model will develop a composite factor of safety for the entire structure under static and seismic conditions. So far, he believes that during a seismic event, the left abutment would not likely move, but estimates the right abutment would move between 8 and 20 inches. However, this is based on very limited information about the subsurface conditions, which could make a big impact on the results. He recommended four more borings be drilled from the crest of the dam on the right abutment to find out the makeup and strength of the substrate. This information will allow him to reanalyze the stability of the dam and design any necessary repairs.

Councilor Warr asked if the right abutment would crack or slide forward during a seismic event. Mr. Heslin said he did not know, but the dam safety engineers would require further analysis because the problem is not fully understood.

Councilor Price said this project has already cost close to \$400,000. She asked if it was possible to mitigate a dam of this age and structure against a 9.0 Cascadia event and if so, what would it cost. She also wanted to know the size and depth of the borings and whether the borings would weaken the structure even more. Mr. Heslin stated the borings would not weaken the dam. The hole will be three inches in diameter and 150 feet deep. He believed the dam could be mitigated against a Cascadia subduction zone earthquake, but was unsure of the costs because the problem is not fully understood. If stability of the dam is inadequate, it is likely a substantial structure would need to be built downstream to provide additional buttressing, which would be expensive.

Councilor Herzig understood that Senator Bonamici has requested pre-disaster funding from Federal Emergency Management Agency (FEMA). He believed funding like this could help Astoria mitigate against events before lives are lost. He also noted the Consent Calendar included an item that would move \$20,000 from the Phase 3 budgeted amount back to Phase 2 to complete this study. He confirmed this would be discussed. He hoped Phase 2 would reveal that Phase 3 is unnecessary. Engineer Harrington explained that Phase 2 was the seismic stability analysis, which Staff hoped would answer all of the questions about the dam. Phase 3 is a peak maximum flow study, which is the last thing required by the State. The flow study will analyze what a flood event could do to the dam. Astoria has never experienced overtopping, but the dam did handle smaller events well. Overtopping can scour out the abutment materials and cause the dam to fail, which will be studied in Phase 3. Phase 3 can be postponed, allowing funds to be spent on completing Phase 2. He added that other strategies

might provide mitigation, like a lower dam elevation level. However, Astoria's current summertime storage issues make this option impossible. The State might be willing to discuss keeping the level low for eight or nine months of the year to minimize risks. Higher levels when soils are drier might result in a better outcome during a Cascadia event. Staff hopes resiliency funding could be used on projects like this one.

Councilor Herzig understood Phase 3 was not mitigation; it was to study a separate issue. Engineer Harrington explained the phases had been renamed for budgeting purposes.

Mayor LaMear invited the public to speak about the Bear Creek Dam.

Chris Farrar, 3023 Harrison Avenue, Astoria, liked the idea of lowering the water level behind the dam. He asked if historical information about the dam included the details of raising it and whether raising the dam's height is part of the problem. The top is very weak because it is not tied in well to the rest of the dam. The dam could completely collapse because of the way the top is held into the formation on either side. However, the lower portion of the dam seems to be completely embedded into the basalt. He believed surface geophysical methods could be used to define the amount of basalt and sandstone on the right embankment.

Mr. Heslin said the raised portion of the dam is structurally sound at its connection with the original dam. The structural engineer conducted a modal analysis of the concrete and confirmed the dam would behave as a monolith. Geophysical methods in areas where the ground and rock are sloped steeply will give very poor results. In order to determine the strength of the material, samples must be collected and sent to a lab. He did not believe geophysical methods were good options for this site. Money would be best spent on drilling for samples of rock underneath the dam.

Carin Temple, 1032 Grand Avenue, Astoria, said she lived in the Bear Creek Valley for 25 years and has a historic perspective of this situation. It is incredible that several decades after the first study the dam has not been improved. The dam was in the foreground of public discussion at that time. She was surprised that the presentation did not include a portrait of the dam, just an aerial view. Anyone who has been to the dam and has seen the crack in the middle probably would not be able to sleep at night. She carried a very high flood insurance policy when she lived in the valley. City Staff came to her house to show her what would happen to her property and animals during various types of earthquakes. This information made her decision to move easier. She wanted Council to know what residents went through this several decades ago and said it is high time something was done about the dam.

Engineer Harrington said Ms. Temple made a good point. Her comments could be made about Astoria's entire infrastructure. Recently, Astoria has been blessed with funding for bridges and progress is being made. However, this dam is an overwhelming problem. The bridges and the dam make the bulk of Astoria's big structures and all but one bridge has been funded, so, repairing the bridge would be a significant milestone for Astoria.

Item 2(c): Parks Master Plan Update

In May of 2015, Astoria City Council adopted "develop a city parks master plan" as a goal for the 2015-16 fiscal year budget. The Parks and Recreation Department has begun preparing a Comprehensive Master Plan, with the assistance of RARE AmeriCorps participant Ian Sisson. This plan will establish short and long-term priorities for the maintenance, improvement, and future development of the Parks and Recreation Department's parks, facilities, and program offerings. The planning process will involve a comprehensive inventory and assessment of existing assets and recreation opportunities, a level of service analysis based on state and regional benchmarks, a set of prioritized recommendations, and a strategy for implementation.

A critical aspect to the success of this project is the engagement of the community in the process, in a way that is equitable and inclusive of the needs and interests of all Parks and Recreation user groups. As part of this effort, Mayor LaMear has appointed a Citizen Advisory Committee to oversee the planning process. This group will hold its first meeting the week of October 19, 2015. On Tuesday, November 10, 2015 from 6:00 - 8:00 p.m., the first in a series of public workshops will be hosted by the Parks and Recreation Department, in the Lovell Showroom at Fort George Brewery. This event will allow the department to engage the community at large, provide further information on the planning process and goals, collect information related to the community's use

of and priorities for Parks and Recreation's offerings, and begin the process of creating a shared community vision and set of priorities for the future of Astoria's Parks and Recreation Department.

Director Cosby said in order to achieve City Council's goal of developing a Parks Master Plan, \$35,000 was allocated to completing the plan this fiscal year. Astoria contracted with Resource Assistance for Rural Environments (RARE) and hired Ian Sisson just over a month ago. Throughout the process of developing the plan, Staff will continue to update City Council on the progress.

Ian Sisson, RARE AmeriCorps Program, stated the last Parks Master Plan was created in 1978. The Parks Department has indicated it is inefficient for them to move forward without a set of priorities. This planning process will allow the City to revisit the community's priorities. He explained that the RARE program is an AmeriCorps program administered by the University of Oregon as part of their Planning and Public Policy Graduate School. The program's mission is to increase the capacity of small communities. He would work in Astoria for 11 months preparing the plan. Director Cronin is an alumnus of the RARE program, and the Astoria Downtown Historic District Association (ADHDA) and Clatsop County have utilized the services of RARE participants. The basic framework of the Parks Master Plan is to evaluate what facilities, parks, trails, and programs Astoria currently has, identify Astoria's needs, and create a strategy to implement Astoria's priorities. The number one objective is to have substantial citizen involvement because it is the most important part of the planning process. The plan will include an inventory, an existing conditions assessment, and a set of recommendations with priorities and costs. The project scope will include a background, community engagement process, the inventory, recommendations and costs, and an implementation strategy. He displayed an outline of the document on the screen and explained this outline would be used to organize the plan. This fall, a community survey will be conducted at several workshops and online. The inventory has already begun, and during the winter, a level of service analysis will be done to compare Astoria's services to regional and national benchmarks. After the analysis is complete, a needs assessment and recommendations will be developed. The community will be surveyed on initial recommendations in the spring, so the recommendations can be refined before submitting the plan to City Council for adoption. Flyers of the first community input session were distributed at the dais and made available to the audience. The first session has been scheduled for Saturday, November 7 at the Lovell Showroom at Fort George, where Staff will begin the visioning process and collect information about how parks are used. The second session, scheduled for Tuesday, November 10, would have the same agenda and activities. The Mayor has appointed a Citizen Advisory Committee, which will meet for the first time on Thursday, October 29 at 8:00 am at the Astoria Recreation Center. The committee will meet monthly throughout the process and all meetings will be open to the public.

Councilor Herzig said the master plan would make the City eligible for more grants, adding it is smart to develop the plan within the 11 months that Mr. Sisson would be working with the City. He suggested community participation be tracked throughout the process so the City can calculate the effectiveness of its community outreach. He wanted to know what percentage of the population was participating.

Mayor LaMear thanked Mr. Sisson and said the City has been looking forward to this project, and is excited to see what comes out of the study.

CHANGES TO AGENDA

There were no changes.

Mayor LaMear announced the agenda included new statements intended to clarify that the public is welcome to respond to any item on the agenda after Council has deliberated. Instead of asking for public comments after each item, she requested audience members raise their hand to show they would like to speak. This was procedure was suggested by Shel Cantor.

CONSENT CALENDAR

The following items were presented on the Consent Calendar:

- 4(a) City Council Special Meeting Minutes of 9/14/15
- 4(b) City Council Minutes of 9/21/15
- 4(c) Boards and Commission Minutes
 - (1) Planning Commission Work Session of 9/1/15

(2) Planning Commission Meeting of 9/16/15

4(d) Bear Creek Dam Seismic Study Contract Amendment and Oregon Water Resources Department Grant Acceptance (Public Works)

4(e) Authorization to Award 11th Street and Franklin Avenue Storm Drain Project (Public Works)

4(f) Contract Amendment for Senior Center Renovation (Public Works)

4(g) Employee Wellness Policy (Finance)

Councilor Herzig requested Item 4(d) be removed for further discussion.

City Council Action: Motion made by Councilor Price, seconded by Councilor Nemlowill, to approve Items 4(a), (b), (c), (e), (f), and (g) of the Consent Calendar. Motion carried unanimously. Ayes: Councilors Price, Warr, Herzig, Nemlowill, and Mayor LaMear; Nays: None.

Item 4(d): Bear Creek Dam Seismic Study Contract Amendment and Oregon Water Resources Department Grant Acceptance (Public Works)

Councilor Herzig said this item is not a budget amendment, but \$20,000 of Phase 3 funds would be dedicated to Phase 2 to study the geologic surround of the dam for seismic purposes. Engineer Harrington added that from the same fund, unused money from another project will fund the rest of Astoria's portion of the costs, with the Oregon Water Resources grant funding the other half of the costs.

Mayor LaMear understood the Spur 14 pipeline project was the project that would be postponed. Engineer Harrington explained the Spur 14 pipeline was for a supplemental water source that would help Astoria's water quality management. The project is not being postponed. However, Staff had anticipated the project would be further ahead. The project is still in the design phase and will not go out for bids until the next fiscal year. City Manager Estes reminded that a contract amendment for the engineering services on this project was approved at City Council's October 5th meeting. Because the design process needs more time, the funds have become available for use this fiscal year. The Spur 14 project will be included in the next fiscal year's budget.

City Council Action: Motion made by Councilor Herzig, seconded by Councilor Nemlowill, to approve the Consent Calendar. Motion carried unanimously. Ayes: Councilors Price, Warr, Herzig, Nemlowill, and Mayor LaMear; Nays: None.

REGULAR AGENDA ITEMS

Item 5(a): Liquor License Application from Jeremy Todd Emmons, doing business as The Paradoxx Bistro & Market, Located at 1820 SE Front Street, for a New Outlet for a Full On Premises Sales License (Finance)

A liquor license application has been filed by Jeremy Todd Emmons doing business as The Paradoxx Bistro & Market, located at 1820 SE Front Street, Astoria. The application is for a New Outlet for a Full On-Premises Sales License. The appropriate departments have reviewed the application and it is recommended that Council consider approval.

Councilor Nemlowill declared a potential conflict of interest as an owner of a business that sells alcohol and beer, Fort George Brewery, but she did not believe this was a direct conflict.

City Council Action: Motion made by Councilor Warr, seconded by Councilor Price, to approve a liquor license application by Jeremy Todd Emmons, doing business as The Paradoxx Bistro & Market. Motion carried unanimously. Ayes: Councilors Price, Warr, Herzig, Nemlowill, and Mayor LaMear; Nays: None.

Item 5(b): Consider Property Sale – 2nd Street (Public Works)

A request to purchase a City-owned lot on 2nd Street has been received from Diana Kirk, Tidal Properties, LLC. Ms. Kirk recently purchased property located at 258 2nd Avenue and would like to purchase the 4,000 square foot City lot adjacent to her property. The minimum size for a buildable lot is 5,000 square feet. Because this is a substandard lot, it can only be sold to an adjoining property owner who would be required to incorporate this property into their existing tax lot. An appraisal report has been prepared by Appraiser Steven Weed. He

estimates the property to have a real land value of \$14,000. Ms. Kirk has indicated a willingness to buy the property at this value and to pay the additional appraisal cost of \$200, which is above the City's appraisal fee. If Council is willing to consider the private sale, a public hearing will be scheduled for November 2, 2015. It is recommended that the City Council determine whether the private sale of a 4,000 square foot lot to Ms. Diana Kirk, Tidal Properties, LLC, should be considered, and if appropriate, schedule a Public Hearing on November 2, 2015 to address the proposed sale.

Councilor Price said City Council recently discussed the housing problems in Astoria. One of the most common ways to address a housing shortage is to allow development on smaller than standard lot sizes. The standard lot size in Astoria is 50 feet by 100 feet and this lot is 50 feet by 80 feet. The lot is on a steep slope, but is not in a slide zone, and it provides a 180 degree view of the Columbia River. According to the appraisal, there are no encroachments or easements on the property, which is located in the R-3 zone. She believed the lot could be beautifully developed into a single or multi-family dwelling. She believed the City must begin to allow infill on substandard lots in order to address the housing problem. For the price of \$14,000, she wanted to know what Jim Tierney of the Community Action Team could do with the lot. Mr. Tierney had stated lots like this one as low hanging fruit that the City could control and use to add to the housing inventory. She explained the lot is ideally situated within easy walking distance of downtown, adding it and others like it are valuable to the City's finances if marketed at development prices. She believed the City should not consider any offer on substandard lots unless the offer is accompanied by a development proposal or until a decision has been as to how to address the housing problem.

Councilor Herzig appreciated Councilor Price's comments, but believed her comments should be part of the public hearing. Councilor Price said she would be happy to repeat her comments later.

Councilor Nemlowill stated Ms. Kirk had contacted her about access issues to her property. The street is an unimproved City street where residents of the nearby apartment complex park, blocking access to Ms. Kirk's property. Access should be discussed at the public hearing.

City Council Action: Motion made by Councilor Warr, seconded by Councilor Nemlowill, to schedule a Public Hearing on November 2, 2015 to address the proposed sale of a City-owned lot on 2nd Street to Diana Kirk. Motion carried unanimously. Ayes: Councilors Price, Warr, Herzig, Nemlowill and Mayor LaMear; Nays: None.

NEW BUSINESS & MISCELLANEOUS, PUBLIC COMMENTS (NON-AGENDA)

Fred White, 2011 Irving Avenue, Astoria, said he had spoken before at City Council and Planning Commission meetings about the three second walk signals on Commercial and Marine Drive, which meant people like himself, in his 70s, had no time to even think about walking across the intersection. A pedestrian is not able to get half way across the street, walking at a good pace, in three seconds. He understood Oregon Department of Transportation (ODOT) owned the signals, but the citizens own the city. He encouraged City Council to pressure ODOT to do something about this situation. Years ago, ODOT promised to improve the downtown signals, but all they did was give pedestrians three seconds. He believed that with computers, the time could be corrected without changing traffic flows, which is ODOT's biggest priority. The Irving Avenue Bridge will be completed in two weeks or less and he was concerned about pedestrian, skateboard, and bicycle safety. Many people are eager to use the bridge when it reopens. The new bridge looks like a highway and he has been concerned about speeding on Irving the entire time he has lived in Astoria. He asked that the Police Department have some presence on Irving when the bridge is first reopened, so people know the speed is only 25 miles per hour. Having a police presence was particularly important when the high school let out, around 1:00 pm or 1:30 pm, and the 16 year olds with new licenses try out Irving Avenue. The low speed and high demand to use the bridge will affect a woman named Roberta, who lives near Harrison Circle and frequents Safeway. She rides her electric wheelchair to Safeway about two or three times each day. About a year ago, she was hit by a car at 33rd and Marine Drive and was badly injured. Now, she has flags on her wheelchair. She must travel along the street because the sidewalks are in such bad condition. However, even if the sidewalks were in good condition, people would continue to park on the sidewalks on either side of 33rd Street. She will need help when the traffic increases on 33rd when the bridge opens in two weeks. He walks everywhere and is aware of how bad the sidewalks are in Astoria's neighborhoods. Connecting sidewalks to the east and west of the bridge, which is not part of the project, are almost impossible to navigate. The new five foot sidewalks on either side of the bridge will meet sidewalks that do not exist for the most part. On the south side of Irving Avenue at the west end of the bridge, the sidewalk is covered with mud and is impossible to walk on in the winter time. This forces pedestrians

to walk in the street. Sidewalks on the north side of Irving Avenue on this side of the bridge are so broken up that a wheelchair would have a difficult time. There is no sidewalk on the east end of the bridge on the north side. On the south side, the sidewalk between the bridge project and 20th Street has been broken up by multiple water main breaks. When the water mains broke under the sidewalk, the concrete was dug up and poorly re-paved. The sidewalk was recently ripped out to accommodate the bridge project and the City's contractors told him there was no money to fix it. He invited all City officials to the grand opening of the bridge. He also asked City officials to park on 16th Street and walk to the bridge so they can see what the residents have to contend with. Astoria needs to start dealing with sidewalks because the entire city is in bad shape. He understood homeowners were responsible for the sidewalks in front of their properties, but the City has been negligent at enforcing the sidewalk codes. This is the 25th year of the Americans with Disabilities Act and it is time the City starts getting serious about it. He believed enforcing the codes city-wide might make last year's City property sales look like a cake walk, but the City has to start somewhere. He suggested a sidewalk inspection be required when a property is sold, sidewalk repairs be required for building permits over several thousand dollars, or that the City begin educating homeowners about their sidewalk responsibilities. He would like the ability to walk around Astoria until he is in his 80s.

Mayor LaMear confirmed Mr. White believed there was not enough time to walk across Marine and Commercial. Director Cook noted there was an additional eight seconds before the traffic light actually changed, but agreed the pedestrian light design was very confusing. Mr. White understood that the traffic light did not change for 13 seconds, but ODOT should be able to change the pedestrian light without changing the traffic signal.

Mayor LaMear said the City was aware of the sidewalk issues and she appreciated Mr. White's suggestions for handling the problems. Recommended solutions are always welcome. She asked who was responsible for the sidewalks at the ends of Irving Avenue Bridge where there are no homes.

Mr. White understood that the process for getting a sidewalk repaired was for homeowners to complain about their neighbors. Sidewalks are not touched until a complaint has been filed.

Director Cook said that in 10 or 15 years, some development project would bring sidewalks to the bridge. The bridge is being built with sidewalks now to avoid retrofitting in the future.

City Manager Estes believed staff could make some recommendations to City Council and update Mr. White on the discussion. There are some comprehensive Code enforcement policy issues and significant budget issues to consider.

Director Cook noted that when a water line causes a sidewalk to fail, the Public Works Department is committed to reinstalling the sidewalk in as good or better condition.

Councilor Herzig suggested using Promote Astoria Funds as an incentive for property owners in the downtown area to fix their sidewalks. Having a walkable downtown is critical to Astoria's tourist economy, so this might be an appropriate use of those funds.

George McCartin, 490 Franklin Avenue, Astoria, said he walks every day with his dog. The temperature is dropping every day and eventually, the weather will be even more of a burden for those who sleep in the streets. Astoria is lucky to have the warming shelter, which was extremely well received last year. He was sure the shelter would be just as well received this year as long as there are enough funds to finance it. He understood some funds were withdrawn from a group that refused to comply with the funding eligibility requirements. He also understood the City was concerned about constitutional problems that could occur if the present location of the warming shelter is financed. The shelter is currently located in the basement of the Methodist Church. The church is providing the space for the shelter without demanding any money, but he did not believe the church would agree to finance the additional utilities. He believed City Council could come up with a few thousand dollars to help the warming shelter. The *Lemon v. Kurtzman* case of 1971 and many other U.S. Supreme Court cases since then have established that the City is free to provide funds for secular purposes even though it is connected with a church. In order to provide funds legally, the purpose must be secular, the primary effect of the action must not advance or inhibit religion, and the action must not foster excessive government entanglement with religion. He believed Astoria would easily meet those objectives without any constitutional impairment or claims that the City gave money to a church. If this does not persuade the City to provide the warming center with funding, he hoped the community would help. The community has already generously given thousands of

dollars to the monument across the street, the Armory, and the Column restoration. People with excess money could come up with a few thousand dollars here and there to finance the warming center's needs. Believe it or not, homeless people matter.

Steve Fulton, 1050 34th Street, Astoria, said he heard the Irving Avenue Bridge would be opening. With the bridge closed, he must access his house by going past Safeway and turning at 35th and Irving Avenue. After dinner one evening, three people were walking their dogs in this area. Irving Avenue is a major east-west arterial for Astoria and currently, there are only two ways to get to that end of town. Getting to that side of town was a major issue with the heavy traffic this summer. He hoped this would resolve itself when the bridge opens back up, but there will be quite a bit more traffic on Irving Avenue. He wanted to see a comprehensive plan to make the area safer for pedestrians and vehicles. The area needs sidewalks, but he did not like the idea of assessing the land owners because some of the houses were built when Irving Avenue had planked streets. He asked the City Council to consider a different designation from a transportation standpoint. He believed Connect grants could help address this issue. He understood sidewalks were expensive because of curbs and catch basins, but this is an issue the City should consider to make the area more liveable. The lighting in the area is also terrible. Astoria's standard is one street light for each block. However, the standard should be doubled because of the situation on Irving Avenue because it might improve pedestrian safety in the area. He is the only Port of Astoria Commissioner that lives in Astoria. The Port has had some issues with zoning and code applications associated with the Bridge Vista Area of the Riverfront Vision Plan. The agenda for the next Port Commission meeting includes discussion of a vacant property on the river side of The Dunes Motel that shares a tax lot with part of Memorial Park. The Port has received a proposal to lease the property for a hotel. He wanted to let Council know this issue would be coming up at their meeting on October 20 at 6:00 pm at the Gateway Building.

Jeremy Todd Emmons of The Paradoxx Bistro and Market thanked City Council for approving his request for a liquor license.

Councilor Herzig said it was nice to get one more business on the south slope.

Mr. Emmons explained that his business is located at the former Tide Point property at 1820 SE Front Street, about one mile past Fultano's Pizza. The bistro is open between 11:30 am and 8:00 pm Wednesday through Saturday and Sunday brunch is served from 10:00 am to 2:00 pm. However, he was planning to extend the hours and menu options.

ADJOURNMENT

There being no further business, the meeting was adjourned at 8:42 pm to convene the Astoria Development Commission meeting.

ATTEST:

APPROVED:

Finance Director

City Manager

A work session of the Astoria Common Council was held at the above place at the hour of 6:00 pm.

Councilors Present: Nemlowill, Herzig, Warr, Price, Mayor LaMear

Councilors Excused: None

Staff Present: City Manager Estes, Assistant City Manager/Police Chief Johnston, Community Development Director Cronin, Parks and Recreation Director Cosby, Finance Director Brooks, Fire Chief Ames, Library Director Tucker, Public Works Director Cook, and City Attorney Henningsgaard. The meeting is recorded and will be transcribed by ABC Transcription Services, Inc.

VISION/STRATEGIC PLAN

Mayor LaMear said Erik Jensen would discuss the differences between vision planning and strategic planning to assist Council in providing direction to staff because the City needs some long-range planning in addition to the Council's annual goals.

Erik Jensen, Jensen Strategies, described the services offered by his consulting firm and noted his experience with visioning processes and strategic planning. His PowerPoint presentation was as follows:

- Community visioning and strategic planning were similar in that both start with the big picture and work down to the on-the-ground activities that make the big picture come to life.
 - Both are also built on stakeholder engagement; the more stakeholders that get involved, the more those stakeholders participate.
 - Both are long term, interdisciplinary planning processes that are proactive instead of reactive.
 - These planning processes also share established goals and objectives, and both offer a framework for policy and operational decisions.
- The differences between community visioning and strategic planning were described as follows:
 - Community visioning is defined as a process of identifying what a community should look and feel like by a certain period of time, usually 20 to 30 years out. The community decides what it wants, so the City can move towards that. Strategic planning is organizationally focused and the entire staff is involved in the process, usually for a 5 year timeframe or less.
 - Stakeholders for both processes include key external interests like the County, Chamber, hospital, elected/appointed officials, and City staff; however, these stakeholders are engaged differently for each process.
 - Though the action plans for each are structured similarly, the content of the plans change depending on the focus. Visioning will have more external activities and actions, and strategic planning will have an internal focus on actions.
 - Both processes inventory current issues and trends, set goals, establish necessary actions needed to achieve the goals, and define how the actions will be implemented. However, the end results of each process will be different.
- How does a community vision relate to a strategic plan? He displayed a pyramid, which he used to explain the process, starting at the top of the pyramid. The Vision gives a big picture of what the community wants. The Organizational Mission is a succinct statement. The Core Values are the values and characteristics employed when doing the work of the mission. The Goals and Objectives are the outcomes of the mission. And the Strategies and Actions are last, at the bottom of the pyramid.
- Community visioning is done to build a community identity, hear the community's vision for the future, build the community's ownership in shared visions, and foster partnerships and joint ventures. Visioning results in a vision statement and action plan and can provide validation of what the City is already doing right.
- In strategic planning, the plan provides organizational direction and serves as a policy guide for decision making, assists staff as they make operational decisions, saves time and money, links long-term direction with short-term actions, and clarifies the organization's intent, which is tied to budget performance. Planning results in a mission, core values, objectives and goals, a strategy for each objective, and actions for each strategy. Planning could also reaffirm existing missions and core values.

- The mission statement is a statement of purpose and responsibilities for the organization. Core values make up the philosophy that guides the organization in achieving its mission. Objectives and goals are the desired outcomes.
- Referring to a pyramid that listed potential stakeholders that should be involved in each level of the processes, he explained how to engage each group of stakeholders at each level, noting the following:
 - Effectively implementing an action plan for both processes requires an entity to take ownership of the action. Each action should identify a party responsible for moving the action forward, not necessarily paying for the action, which causes a lot of confusion sometimes. Each action should also have a timeline.
 - It is important to consider time and cost. More stakeholder engagement will result in a longer process, but also a more sustainable plan. This will cost more time and/or money up front. The more people involved in the process, the better the opportunity for success of the plan. His recommended best-case scenario was to get stakeholders involved as suggested on the pyramid and spend between 6 and 18 months on visioning and 3 to 12 months on strategic planning.
- Success factors for both processes are stakeholder involvement, connectivity and consistency throughout each document, flexibility to modify the documents without impacting the integrity of the plans, leadership and advocacy among City Council and staff, and a framework for implementation.

Mayor LaMear asked how active City Council should be for both processes. Mr. Jensen said Council's external role in the visioning process is to encourage the community to participate. City staff should also encourage community participation. Additionally, when staff is not involved in the process at the right level, staff cannot take ownership in development of the vision. Staff and Council must have a mechanism for submitting input into a broad pool of ideas, with that input being processed equally alongside the ideas from the community. Strategic planning involves policy direction and City Council is the policy making body for the City. Therefore, once data has been gathered, Council's input is important. The leadership team and staff must also participate within the context of the work that they normally do.

Councilor Herzig said Clatsop County recently hired a consultant to assist with their visioning process. The consultant hosted several focus group sessions throughout the county and reported back to the County Commission. However, he believed the County did not go as far down on the pyramid as they should have. The consultant submitted a general statement, only about five or six pages long, to the County Commissioners. The Commission adopted the document as their vision plan, but there was no implementation process. He believed implementation was essential.

Councilor Nemlowill said she appreciated Mr. Jensen's presentation and believed it made a lot of sense. Astoria has a little bit of experience, as the City has been implementing the Riverfront Vision Plan, which was adopted in 2009. The Riverfront Plan is challenging to implement because so much time has gone by since it was adopted. She believed an important part of implementation is that it occurs when ideas are still fresh in the community's mind. She asked if the action plan of a vision statement should specifically relate to a strategic plan or a separate document. Mr. Jensen said the vision statement's action plan needs to be consistent with the strategic plan. Most of the time, the City is responsible for most of the action items on the action plan. Therefore, it is important to marry those action items with the strategic plan so there is consistency between the two documents. The vision should provide an overall guidance on the strategic planning process, but should not be comprehensive because the strategic plan will be much more comprehensive to the City and the services it provides.

Councilor Nemlowill asked how a City would ensure connectivity and consistency between existing documents, like the Comprehensive Plan, which includes a lot of visioning language. Mr. Jensen explained that other long-term plans will exist, like a Parks Master Plan or Capital Improvement Plan. Those plans will eventually have to marry with the vision. Comprehensive Plans are updated every so often, which provides the opportunity to connect the two documents and make them consistent with each other. City Manager Estes added that the vision plan has a multi-year implementation time frame and updating several other plans may take a number of years.

Councilor Nemlowill asked how to get stakeholder involvement in an authentic manner, as opposed to simply compiling the responses of large numbers of people. She also wanted to know how the responses should be synthesized to reflect a true statement of the community's vision. Mr. Jensen said in his experience, it has been helpful to have a Council appointed task force or committee tasked with wording the vision. It is key to ensure

the committee does not operate in a vacuum because it will use information it gets from the community to draft the vision statement. Meaningful public involvement requires the City to demonstrate it is truly listening to what the community and stakeholders say. This is achieved through documentation and by reminding the community throughout the process that their ideas have been heard. This does not mean the City must accept every idea that comes forward. All of the ideas should be put through a filter that helps build the vision while the City demonstrates that all ideas have received serious consideration along the way.

Councilor Price thanked Mr. Jensen. She had seen the Hillsboro 2035 Plan, which was built upon the Hillsboro 2020 Plan. It is an impressive and comprehensive multi-page document; however, Hillsboro has no strategic plan. She wanted to know how Hillsboro viewed the difference between a strategic plan and a visioning plan. After hearing Mr. Jensen describe the process and outcome of a strategic plan, she believed a document created by the City of Albany would make a good template for Astoria. She was most interested in a strategic plan because Astoria has never had one. A strategic plan seems to be such an effective communication tool for current and former City Councilors, citizens, and residents. A vision plan could be created at the same time as a strategic plan, but it would take longer and the two documents would need to be melded together somehow. Mr. Jensen responded it is difficult to create both documents simultaneously because the two documents are created in silos and may or may not mesh. If the documents do not mesh, trying to sort that out could take a long time.

Councilor Price wanted to know where a city with neither document should begin. Mr. Jensen said in an ideal scenario, a city would begin with a vision plan and then move on to a strategic plan. Most of the jurisdictions he works with do not have this luxury because of budgets and timelines. The City will have to consider its needs and priorities. A vision plan can be created after a strategic plan, but it is not ideal. A hybrid document could also be created by combining the two processes, which might take longer than each individual process would normally take. Again, this is not ideal, but the City should do what works for its situation.

Councilor Warr said initially, he was not in favor of having a strategic plan. However, the more he learned about it, the more he became convinced it would be a good way to go. He appreciated Mr. Jensen's input.

Councilor Herzig noted Councilor Nemlowill's concern about getting community input. When the County was going through its visioning process, the consultant had a difficult time engaging the community. He attended at least three public sessions and the consultant was happy when 10 people showed up. He has found with a number of processes that it is difficult to get enough people to show up. When he toured Redmond with the League of Oregon Cities, the community development director spoke about the importance of celebrating every step along the way with a community celebration of each milestone. If Astoria takes the community's input, the City needs to show them their input is being implemented in a timely fashion, not five or 10 years down the line. He referred to a PowerPoint slide that displayed an ascending arrow with time and costs. One reason Astoria wanted to consider a vision and strategic plan was because they hoped the arrow would be descending. The City believes the correct flow down process will expedite strategic decisions and staff operations. He asked if Mr. Jensen meant for the arrow to continue to go up. Mr. Jensen answered no, the arrow represents the development process for either plan, and not what happens after one of the plans has been implemented. He agreed that the plans should bring costs down, but confirmed that there would be an initial investment of time, energy, and costs. However, as planning takes place, those investments should decrease.

Mayor LaMear agreed with Councilor Nemlowill that the Riverfront Vision Plan has taken so long to implement that the community is now saying it has a different vision. The City must tread carefully in order to get enough input without dragging the process out so long that the vision changes. Mr. Jensen said the scenario Mayor LaMear described is common. Local governments in Oregon face a barrage of issues that must be dealt with on a regular basis. It is easy to lose focus even on a large process like a vision plan because so many other things are going on. The momentum of the process is very important to a plan's success. He advised the City to set up their plan so it can be implemented immediately upon completion. Hillsboro set up a citizen oversight committee responsible for moving the vision plan forward and reporting to City Council once a year on the progress of the vision. The committee was made up of many of the partners who were involved in the implementation of the plan.

City Manager Estes added that both processes would require a periodic update process to prevent implementation from going stale. Mr. Jensen suggested minor updates be given annually, and then every five years, hold another public involvement process to check in with and update the community on a larger scale.

Mayor LaMear thanked Mr. Jensen for his presentation, which was very helpful.

ADJOURNMENT TO REGULAR SESSION

There being no further business, the work session was adjourned at 6:58 pm to convene the regular session Astoria City Council meeting.

ATTEST:

APPROVED:

Finance Director

City Manager

DRAFT

HISTORIC LANDMARKS COMMISSION MEETING

City Council Chambers
September 15, 2015

CALL TO ORDER – ITEM 1:

A regular meeting of the Astoria Historic Landmarks Commission (HLC) was held at the above place at the hour of 5:15 p.m.

ROLL CALL – ITEM 2:

Commissioners Present: President LJ Gunderson, Commissioners Jack Osterberg, Paul Caruana, Mac Burns, Kevin McHone, and Thomas Stanley

Commissioners Excused: Vice President Michelle Dieffenbach

Staff Present: Community Development Director Kevin Cronin, Interim Planner Mike Morgan, Special Projects Planner Rosemary Johnson, Parks Director Angela Cosby, and Police Chief Brad Johnston. The meeting is recorded and will be transcribed by ABC Transcription Services, Inc.

APPROVAL OF MINUTES – ITEM 3:

Item 3(a): Minutes of July 21, 2015

President Gunderson asked if there were any changes to the minutes of July 21, 2015. There were none.

Commissioner Burns moved to approve the minutes of July 21, 2015 as presented; seconded by Commissioner Caruana. Ayes: President Gunderson, Commissioners Caruana, Osterberg, Burns, Stanley, and McHone. Nays: None.

Item 3(b): Minutes of August 18, 2015

President Gunderson noted the date in the footer should be corrected to August 18, 2015.

Commissioner Osterberg moved to approve the minutes of August 18, 2015 as corrected; seconded by Commissioner Caruana. Ayes: President Gunderson, Commissioners Caruana, Osterberg, Burns, Stanley, and McHone. Nays: None.

PUBLIC HEARINGS

President Gunderson explained the procedures governing the conduct of public hearings to the audience and advised that the substantive review criteria were listed in the Staff report.

ITEM 4(a):

EX15-11 Exterior Alteration EX15-11 by Daric Moore, Daric Moore Building Arts to convert the lower front portion of the basement into a covered porch at 842 Irving in the R-3, High Density Residential zone.

President Gunderson asked if anyone objected to the jurisdiction of the HLC to hear this matter at this time. There were no objections. President Gunderson asked if any member of the HLC had a conflict of interest, or any ex parte contacts to declare. None declared. President Gunderson requested a presentation of the Staff report.

Interim Planner Morgan presented the Staff report and recommended approval with conditions.

President Gunderson opened public testimony for the hearing and confirmed the Applicant was not in attendance to give a presentation. She called for any presentations by persons in favor of, impartial to or against the application. Seeing none, she confirmed there were no closing remarks from Staff. She closed the public testimony portion of the hearing and called for Commission discussion and deliberation.

Commissioner Osterberg agreed with the Staff report and believed the application met all of the criteria. The proposal will result in a substantial improvement to the existing conditions, which are not historic.

Commissioner Stanley said he was pleased that the property owner is making the effort to fix up the house. The work will add a lot to the neighborhood.

Commissioner Caruana said he was glad Staff noted the windows and doors would be trimmed because the scale of the moldings is not always included in the information given to the Committee.

President Gunderson said if the Applicants were present, she would thank them for using historically accurate windows and doors.

Commissioner Stanley moved that the Historic Landmarks Commission (HLC) adopt the Findings and Conclusions contained in the Staff report and approve Exterior Alteration EX15-11 by Daric Moore; seconded by Commissioner Burns. Motion passed unanimously.

Interim Planner Morgan added that Irving is a high traffic corridor through town and he believed it was great to see renovations in areas of town that have many visitors.

Commissioner Stanley said the number of houses that have been restored since he first served on the HLC has been staggering and Astoria is becoming more beautiful.

President Gunderson read the rules of appeal into the record.

Interim Planner Morgan excused himself from the meeting at this time.

ITEM 4(b):

NC15-03 New Construction NC15-03 by Verizon Wireless LLC dba Verizon Wireless to construct a 150-foot wireless communication facility adjacent to a structure/site designated as historic at 1580 Shively Park Road in the IN, Institutional zone.

President Gunderson asked if anyone objected to the jurisdiction of the HLC to hear this matter at this time. There were no objections. President Gunderson asked if any member of the HLC had a conflict of interest, or any ex parte contacts to declare.

Commissioner Caruana declared that Verizon is a tenant on the Astor Hotel building downtown. He confirmed this may affect his decision and stepped down from the dais.

President Gunderson declared she was a customer of Verizon Wireless, but this would not affect her decision.

Director Cronin noted that Verizon has a contract with the City of Astoria.

President Gunderson requested a presentation of the Staff report.

Special Projects Planner Johnson presented the Staff report and recommended approval with conditions. Staff received a letter of opposition from Ron Zilli, which was available at the dais.

President Gunderson asked if the City used cell phones for its emergency communications and if so, was the service from Verizon. Planner Johnson said Astoria's emergency communications were not serviced by Verizon and the emergency communication facilities will be located on a tower proposed at the Land Reserve east of the Column above the old reservoir. The tower in this request will only serve citizens, not emergency services.

Police Chief Brad Johnston, 2828 Grand, Astoria, explained this project began in 2006 and emphasized that this proposal was driven City Council, not the Friends of the Astoria Column. At the April 1, 2013 City Council meeting, Staff originally proposed that the tower be built on Coxcomb Hill. The Friends were at this meeting to discuss their master plan and were obviously concerned with the City's proposal to put a tower on Coxcomb Hill. However, City Council requested another location be chosen for the tower site. The ideal location for Verizon would be at the top of the hill by the picnic structures. However, this site would have been highly visible.

Therefore, the proposed location is the best possible satisfactory location. Astoria's primary emergency communications are land mobile radio devices, which include very high frequency (VHF) walkie-talkies and car radios. Astoria does use Verizon for data services and relies on cell phone services for confidential communications. Despite the information in the letter of opposition, this site will not co-locate any emergency communications equipment and will only be used for wireless communications from commercial providers. He confirmed that having a good Verizon signal in Astoria would still benefit the City's emergency services. This tower is part of a larger project to improve the City's emergency communications. While the site is not directly critical to Astoria's emergency communications, it is a necessary component of a deal to move the City's primary communications facility to the Reservoir site and allow Verizon to complete a system upgrade. The City's proposal to put the primary communications site at the Reservoir will be reviewed by the Planning Commission on September 16, 2015.

Commissioner McHone was concerned about the ability of other commercial service providers to co-locate on the tower. There is a lack of technically viable locations to put the cell towers within the community. He asked for details about how the lease was structured. Staff explained the lease is structured to require Verizon to allow co-location by other commercial providers and to ensure those providers request access rights to the tower from the City of Astoria. The Development Code requires co-location. When a provider wants to install facilities in Astoria, they are first required to consider stealth installations, like in a steeple on a church. If that is not a viable option, the provider would have to install their facilities on an existing tower after proving a stealth installation was not physically feasible. Staff confirmed the tower would be owned by Verizon, located on City-owned property leased to Verizon. The Wireless Communication Facilities permit that will be reviewed by the Planning Commission on September 16 addressed issues including maintenance, removal, and co-location. The City has the first right of refusal if Verizon decides they are no longer interested in the tower. However, if the tower does not make business sense for Verizon, it probably would not make business sense for the City. Enclosures for the equipment will be installed mostly below street grade, but there will not be any buildings involved in this project.

President Gunderson understood the HLC could not dictate specific colors. However, Staff has recommended the equipment enclosure be green or brown and the tower be gray. She believed green or brown would be a more appropriate color for the tower because Astoria has not had many gray skies recently. She asked why the City recommended the tower be gray. Planner Johnson said Staff did consider green or brown for the tower. However, the most visible portion of the tower will be the upper sections. From a distance, one will see the portion of the tower that is up against the sky. Staff believed gray was the best color for the majority of the view. She described the view from the parking lot using the photographs in the Staff report, noting that the tower will not be seen from the parking lot. The tower will become visible at the S curve on the road that extends up the hill.

Commissioner Burns asked how tall the trees were within a 20-foot radius of the tower and how many trees would be removed during its installation. Planner Johnson said the deciduous trees are about 120 feet tall and the fir trees are about 150 feet tall. The tower will be taller than some trees, but some trees in the area will be of a similar height. She used the trees instead of the structures to determine scale and compatibility. The Applicant has been working with Parks Director Cosby to keep tree removal to a minimum.

Parks Director Cosby, 1997 Marine Drive, Astoria, said the plans called for the removal of three deciduous trees and one evergreen tree. She did not know the specific height of the trees to be removed, but noted the trees would be removed because they are located within the footprint of the tower and enclosure area. The specific trees are identified in the site plan, which she believed the HLC had received.

Commissioner McHone asked if branches could be added to the tower to make it look like a tree. Director Cosby said while Staff was considering appropriate tower colors, they also considered a mono-pine tower. After looking at mono-pines installed in other parks, Staff did not believe it would be a good fit. The mono-pines wear differently and do not provide the same aesthetic appeal as other trees in the park. Staff did not want the tower to look fake. Additionally, the tree poles are not the same species as trees in Astoria.

President Gunderson asked Staff for their opinions on a dark brown or dark green tower. Director Cosby said she would prefer brown. Planner Johnson said color is a judgment call.

Commissioner Osterberg asked if Staff considered that Criteria B and C were not applicable, as the criteria relate to the design and consistency with the orientation of adjacent historic structures. The HLC is supposed to review the proposed structure's impact to other historic and adjacent structures. However, it appears as though none of those structures are close enough to be visible from the tower site or vice versa. Staff has gone into a

great deal of Findings regarding consistency and appropriateness with the Park. But, the picnic facilities, pathway, parking area, and other visible features are not structures. He asked why Staff decided to view the trees as structures. Planner Johnson said it was difficult for Staff to address the criteria, which is specifically for a new structure. The only structure in the park that is designated historic is Shively Hall. The picnic areas are newer and not part of the historic structures, but they are adjacent to the proposed tower site. Staff considered that the structures would not be visible and were not adjacent, so it would be difficult to define them as compatible. Since the Park is designated historic, she decided to use the entire Park as part of the criteria. The Development Code may not have the exact language to address a cell tower, so she made the best Findings she could based on the criteria.

Commissioner Osterberg understood he would have to take into account the status of the structures in the Park and how well they apply to the criteria. The Staff report does note "as applicable." He believed Criterion D referred to a Section of the Development Code that did not apply to this application. Planner Johnson explained that the section referred to in Criterion D would be reviewed by the Planning Commission. However, it does reference historic review, so she added it for the HLC to consider as well. She wanted the HLC to know the environmental historic review had been approved by the State Historic Preservation Office (SHPO). Criterion D was added to the Staff report for informational purposes.

President Gunderson opened public testimony for the hearing and asked for the Applicant's presentation.

Sharon Gretch, 31649 Sexton Road, Philomath, OR said Planner Johnson did such a good job with the Staff report that her presentation would be redundant. However, she could answer questions. She also had some technical information about why Shively Park was chosen for the tower site, which has a lot to do with the removal of the tower at the Column. Without a tower at the Column, three other sites will be necessary to provide coverage to Astoria and improve services. She showed slides of maps of the current coverage supplied by the tower at the Column, coverage left when the tower at the Column is removed, and coverage supplied with the addition of the tower at Shively Park and all of the towers in Astoria, Warrenton, and Gearhart. Towers must be built at Shively Park, the Astor Hotel, and the Reservoir in order to replace the coverage lost by the removal of the tower at the Column. Since the City will have most of its facilities on the Reservoir tower, Verizon had to place its facilities on other towers in order to provide coverage to the area. Therefore, the tower at Shively Park is critical.

Commissioner McHone asked how many Verizon towers were in Clatsop County. Ms. Gretch said there were at least four, plus the tower at the Column. She added that Verizon advocated for a gray tower because in their experience, gray tends to blend in much better with the background. Gray also wears better as time goes on. Brown will stick out with all of the green coverage in the area. The tower will be painted brown if the HLC required it; however, from Verizon's experience, brown may not be the best choice.

President Gunderson called for any presentations by persons in favor of, impartial to, or against the application. Seeing none, she called for closing remarks of Staff. There were none. She closed the public testimony portion of the hearing and called for Commission discussion and deliberation.

Commissioner McHone said he appreciated all of the effort and years of service that have gone into making this decision. The towers are difficult to locate and no one wants them in their backyard. Considering the lack of locations and all of the groups involved in making this decision, he believed it was a good resolution, which he supported.

Commissioner Burns agreed. Many different parties have done a lot of due diligence and the tower has to go somewhere. Shively Park seems like a logical place to put the tower, so he was fine with the request.

Commissioner Stanley said it seemed that the City and Verizon have done a terrific job of putting the package together for the HLC and he was fine with a gray tower.

Commissioner Osterberg said he supported the application because it meets the criteria for approval and he agreed with the other Commissioners. Criterion B and C have only been broadly interpreted by Staff, which makes it easy for him see that the criteria have been met by the application. There are no visible structures near the proposed tower site, which leaves the HLC to review the impact to the general nature of the Park. There are improvements at the Park, but improvements are not structures. Criterion D is not listed as criteria that the HLC must review and is just for information only. Several items in the Staff report will be reviewed by the Planning Commission, but he appreciated that they were mentioned at this meeting. The letter from Mr. Zilli addresses a

blend of different criteria, some of which are reviewed by the HLC and some of which pertain to other sections of the Development Code. However, the Planning Commission should review Section 15 (Wireless Communication Facilities Ordinance) of the Development Code. He appreciated the items that the letter correctly brought before the HLC, but Mr. Zilli's comments have been adequately addressed in the Staff report. Staff has adequately determined that the tower will have a minimal impact on the surrounding area. He thanked Mr. Zilli for writing such a thoughtful and detailed letter that cited the criteria. He reminded that if this request is approved as proposed, the equipment enclosure and fencing would be painted a dark color, as per Condition 3 of Approval. He believed all of the Commissioners agreed with this. In his experience reviewing wireless communication facilities, the most appropriate way to color a tower has been to use a dark color on the bottom portion and a silver or gray on the upper portion. This allows the tower to blend in from various viewing angles. However, he did agree with the Staff report.

President Gunderson said she fully supported the request. Moving the City's emergency services where they need to be is very important. She deferred to Ms. Gretch with regard to the color because she is the expert in the field.

Commissioner Osterberg moved that the Historic Landmarks Commission (HLC) adopt the Findings and Conclusions contained in the Staff report and approve New Construction NC15-03 by Verizon Wireless LLC, with the Conditions listed in the Staff report; seconded by Commissioner Stanley. Motion unanimously approved.

President Gunderson read the rules of appeal into the record.

Commissioner Caruana returned to the dais.

REPORTS OF OFFICERS/COMMISSIONERS – ITEM 5:

Director Cronin updated the Committee on his work streamlining the Development Review process, noting that he planned to present findings and recommendations to the Commission in October. He also gave an update of the Affordable Housing Study and said he would like to schedule a work session with the HLC to get their input on the housing situation in Astoria.

President Gunderson asked for an update on the search for a Planner. Director Cronin said Staff has already scheduled about nine interviews and he hoped to conduct second interviews in the next couple of weeks. He planned to have a new Planner by November.

Commissioner Osterberg said he once hired a planner because he was the only applicant who had taken an application to the Land Use Board of Appeals (LUBA). This applicant had the experience of working with a City Attorney, appealing a City Council decision, and taking the application through the LUBA process. This experience differentiated him from the other applicants.

PUBLIC COMMENTS (Non-Agenda Items) – ITEM 6:

This item was not addressed.

ADJOURNMENT:

There being no further business, the meeting was adjourned at 6:22 p.m.

APPROVED:

Community Development Director

ASTORIA PLANNING COMMISSION MEETING

Astoria City Hall
August 25, 2015

CALL TO ORDER:

President Pearson called the meeting to order at 6:30 pm.

ROLL CALL:

Commissioners Present: President David Pearson, Vice President McLaren Innes, Kent Easom, Sean Fitzpatrick, Daryl Moore, Jan Mitchell and Frank Spence

Staff Present: Interim Planner Mike Morgan, Community Development Director Kevin Cronin
The meeting is recorded and will be transcribed by ABC Transcription Services, Inc.

APPROVAL OF MINUTES:

President Pearson asked for approval of the minutes of the July 28, 2015 meeting.

Commissioners and Staff noted the following corrections:

- Acting President Innes should be changed to Vice President Innes throughout the minutes.
- Page 2, paragraph 9, next to last sentence – Commissioner Mitchell's name was misspelled.
- Page 2, paragraph 9, last sentence – The word "the" was misspelled.
- Page 3, paragraph 2, first sentence – Interim President should be changed to Vice President Innes.

Commissioner Easom moved that the Astoria Planning Commission approve the minutes as amended; seconded by Commissioner Mitchell. Motion passed unanimously.

PUBLIC HEARINGS:

President Pearson explained the procedures governing the conduct of public hearings to the audience and advised that handouts of the substantive review criteria were available from Staff.

ITEM 4(a):

CU03-04 Permit Extension request for Conditional Use CU03-04 by Elisabeth Nelson for a temporary use permit for one year, to August 26, 2016 to operate the Astoria Conservatory of Music in the existing church structure at 1103 Grand Avenue in the R-3, High Density Residential zone.

President Pearson asked if anyone objected to the jurisdiction of the Planning Commission to hear this matter at this time. There were no objections. He asked if any member of the Planning Commission had any conflicts of interest or ex parte contacts to declare.

Commissioner Fitzpatrick declared a potential conflict of interest. He is a member of the church's congregation, but believed he could make an unbiased decision. He offered to recuse himself. However, the Commission and Staff agreed this was not necessary and Commissioner Fitzpatrick said he would vote.

Commissioner Moore declared a potential conflict of interest. He and his children have taken classes at the Astoria Conservatory of Music. However, he believed he could still vote impartially.

President Pearson asked Staff to present the Staff report and recommendation.

Interim Planner Morgan reviewed the written Staff report. No correspondence had been received and Staff recommended approval of the request.

President Pearson opened the public hearing and confirmed there was no presentation by the Applicant. He called for any testimony in favor of, impartial to, or opposed to the application. Hearing none, he closed the public hearing and called for Commission discussion and deliberation.

Vice President Innes said she fully supported the temporary use because it was a unique source for good training in the community.

Commissioner Fitzpatrick believed the Conservatory made a good neighbor because it does not cause parking issues and the students do not cause problems in the neighborhood. He noted that the space occupied by the Conservatory was built as a church school in 1938 and this is a good continued use for the space.

Commissioner Moore said as a student, he agreed with Commissioner Fitzpatrick and supported the request.

Commissioner Mitchell believed the Conservatory was a great use for the church. Music is something one expects to hear from a church and she would be surprised if there were any complaints from the neighbors. She asked if renewing the permit annually was the best way to handle this situation.

Interim Planner Morgan said he had spoken to former Planner Rosemary Johnson about this. He believed a Code change would be appropriate. Staff could recommend a specific code amendment that would allow this use without causing problems in the rest of the R-3 zones. He confirmed the applicant had to pay a fee each year to apply for the extension.

Director Cronin added that the new City Planner will be tasked with recommending annual code updates to City Council on a regular basis.

President Pearson believed the application met all of the criteria the Planning Commission had been asked to review.

Vice President Innes moved that the Astoria Planning Commission adopt the Findings and Conclusions contained in the Staff report and approve Permit Extension request for Conditional Use CU03-04 by Elisabeth Nelson; seconded by Commissioner Spence. Motion passed unanimously.

President Pearson read the rules of appeal into the record.

ITEM 4(b):

CU15-02 Conditional Use CU15-02 by Frank Linza to operate a bed and breakfast in an existing single family dwelling at 364 Floral in the R-3, High Density Residential zone.

President Pearson asked if anyone objected to the jurisdiction of the Planning Commission to hear this matter at this time. There were no objections. He asked if any member of the Planning Commission had any conflicts of interest or ex parte contacts to declare. There were none.

All of the Commissioners confirmed for Staff that they were familiar with the site. Director Cronin noted the importance of visiting a site, especially when Staff is recommending denial of a request, as being on site puts the application into context.

President Pearson asked Staff to present the Staff report and recommendation.

Interim Planner Morgan reviewed the written Staff report. Staff recommended denial of the request due to inadequate parking. The Applicant recently had a survey completed, is attempting to find additional parking spaces, and therefore has requested a continuance. Extensive correspondence had been received, including letters from two neighbors and a petition containing between 12 and 15 signatures from neighboring property owners. All correspondence has been entered into the record.

Vice President Innes asked if the owners were required to live in the property. Interim Planner Morgan explained that the zoning ordinance requires an owner or manager to live in the bed and breakfast. The Applicant has

stated he does live in the building at 364 Floral. He also owns the property at 366 Floral, but he has certified that he plans to live at 364 Floral and manage the bed and breakfast.

President Pearson asked if Staff had information on combining parking spaces on multiple properties owned by the same person. Interim Planner Morgan said if there was adequate parking on 366 Floral, the Applicant could utilize the space on that property to satisfy parking requirements for 364 Floral. However, the driveway between the two properties only satisfies the parking requirements for the house at 366 Floral. A total of seven parking spaces would be needed for both properties. Therefore, even with the two properties combined parking was still inadequate.

President Pearson opened the public hearing and called for a presentation by the Applicant.

Frank Linza, 364 Floral, Astoria, said he purchased the property at 366 Floral over three years ago and realized the property at 364 Floral was a problem. He told the owner he wanted to purchase the property so he could make everyone happy and had noted the renters would have weekend parties, which created a dangerous situation on Floral. He purchased the property at 364 Floral in January 2015. He and his wife decided the best way to control the traffic to the property would be to control who enters the house and where people park. He renovated the house several months ago and applied for a lodging permit. He has had several guests in the house over the last two months and has received no complaints. No one knows the guests are at the house because he controls the situation. The house is beautiful and represents Astoria. For 30 years, his family operated a business in Aspen, Colorado and worked with that City and tourists. He believed this experience would make him an excellent representative to the City of Astoria. While he had not been able to go door to door, he had spoken with several of his neighbors about his plans to attempt to monitor traffic. Some people believe he is putting in a Super 8, but this is not true. He lives at 364 Floral and his wife owns and lives at 366 Floral. This has worked well so far, but he is a little bit short on parking. The survey was completed on Friday, August 21st and he met with Interim Planner Morgan to discuss options. He believed he had an excellent opportunity to lease property from the City and private landowners within 200 feet of the home. He hoped the Commission would table the discussion until September so that he could show the Commission a lease for at least three parking spaces within 200 feet of the home. If the Commissioners took the time to visit the property, they would understand that the view from the property makes it a spectacular bed and breakfast. In the 3½ years he has lived in Astoria, he has realized there is a huge shortage of housing, especially during salmon season. He is limiting traffic and prohibiting boats and trailers. The house is just a quaint bed and breakfast on a dead end street. He did not plan to have wedding rehearsals or any other big events because he wants to limit the cars on the property to one or two. He wants to keep the area quiet instead of turning it into a million dollar business.

Commissioner Easom asked if Mr. Linza had applied for a lease with the City.

Mr. Linza explained that he had just received the survey at 5:30 pm on Friday, August 21st. He has not had time to meet with the City to discuss a lease. The potential to lease property from the City exists because Floral was supposed to go through to Columbia. Landslide issues prevented the City from going through with that plan. He pointed to the dead end on the Power Point Presentation and explained that the area between Floral and Columbia was unincorporated City property that could be leased for an extended time and he could put in a gravel parking lot. There is also some private land in the area. He was unable to contact the owner over the weekend because the owner is currently out of state. This is why he has requested the hearing be continued for one more month.

Commissioner Mitchell asked if Mr. Linza planned to live next door to the three-bedroom bed and breakfast.

Mr. Linza said he set up separate quarters in a large shop area as his bedroom. His living quarters are separate from the rest of the house at 364 Floral and his wife lives at 366 Floral. He is in Astoria all the time, but his wife is still transferring with her job. She works for Providence Health Care and would like to get a job at Seaside, but a full time opportunity has not yet come available. She is in Astoria on weekends. Her transfer is just a matter of time because she has an interview scheduled at Providence Seaside. He waited patiently for three years to obtain this property. He has seen a huge difference in the flow of traffic. There were four or five renters in the house with no parking available. Since he has taken control of the house, congestion has eased up. His biggest problem is that Google shows that Floral Street goes through. Drivers are constantly turning around and he would like to work out something with the City that allows drivers to turn around easier.

Commissioner Moore said the Staff report indicates lack of parking is the primary reason for recommending denial of the request. The Applicant is suggesting he may have a resolution to the parking issues. He asked if the Applicant's proposal included on-street parking in front of both of the houses as part of the solution.

Mr. Linza said no. He has removed all of the fencing on both properties and installed pavers to provide off-street parking. He preferred the houseguests not use on-street parking. He owns the whole corner, but is two or three parking spaces short of meeting the requirements. He will try his best to obtain these additional parking spaces from the City or private property owners. Parking on the pavers in front of the house adjacent to the street would be included in the total required parking spaces. His drawings show that all of the required parking spaces would be off-street parking. He understood parking is a premium in the area and he has tried his best to get his neighbors to move their junk cars. However, he believed the situation could be controlled.

President Pearson called for any testimony in favor of the application.

William (Jake) Chadney, 350 Floral Street, Astoria, said his house was at the end of the street. After reading an article in the *Daily Astorian* about Airbnb, he suggested Mr. Linza turn 364 Floral into a bed and breakfast. He used to call the property the pirate house when people from Fort George Brewery lived there because they flew a flag with a skull and crossbones. He liked the people that lived there, but the house was a big nuisance because the residents would park near his house and his neighbor's house, making it difficult for them to get in and out. He bears the brunt of what occurs at the end of the street because everyone turns around in front of his house. Even though the street is a dead end, there is no cul-de-sac or provision in the road to accommodate turning around. Drivers usually end up using his driveway to turn around. A bed and breakfast will only be used part of the time instead of 24/7 and provide a way for Mr. Linza to control who is on the property. This is why he supports the application. He knew Mr. Linza had guests at the property a couple of times, but they were not noticeable. There were a lot of trucks at the property during the construction process, but this is to be expected during a renovation. He believed a bed and breakfast would simplify the situation in the neighborhood by having people on the property part-time instead of full-time. He believed that renting the house could cause more problems.

President Pearson called for any testimony impartial to the application.

Tom Gill, 407 Floral Street, Astoria, said he signed a petition opposing the application, but he intended to make impartial comments and pose questions he would like the Commission to consider. He thanked the Planning Commission for their service and for conducting this public hearing. He admired Mr. Linza's work ethic, believed he had great taste, and that he had done a beautiful job on both houses. He understood after speaking with Mr. Linza that the house at 366 Floral was intended as a place for Mr. Linza to retire and the house at 364 Floral was intended for visiting family members. Now, his plans have morphed into this commercial enterprise and he wondered where the plans would go from here. The Staff report indicates the driveway to the east of the property at 364 Floral is 26 feet long. He did not believe two automobiles could fit within 26 feet, except maybe compact vehicles. In the ten years he has lived on Floral, he has rarely seen two vehicles parked in that driveway. When there were two vehicles in the driveway, they almost always overlapped into the right-of-way of the street. The southwest portion of Floral is a very narrow fill for a causeway where two cars cannot pass each other. He was concerned that an obstruction would prevent fire equipment from getting down the street. Any traffic increase would be a matter of concern. The street makes a very tight curve at 407 Floral and extends down a hill to a neighborhood where small children play in the street. There have been a few instances of drivers speeding. He understood that Kingston Avenue has been platted for the space at the end of Floral Street. An attempt to vacate the space was not approved by the City, so he did not know if the space should be considered for additional parking.

President Pearson called for any testimony opposed to the application.

Al Mackey, 372 Floral Street, Astoria, said he was known as the unofficial mayor of Floral Street. He believed the end of the road was the only viable location for a bed and breakfast. A bed and breakfast should not be located along the street because of parking issues and accessibility for emergency vehicles. He asked the Planning Commission to deny the request. He has lived on Floral Street for about 80 years and knew quite a bit about the history of the neighborhood. He told the story of a church, built in the neighborhood in the 1920s, that had to be torn down because it was difficult for people to walk up the hill to get to the church. He had admired the church's engineering and architecture, but it did not have any posts supporting its sides. He reiterated that Mr. Chadney's

house was the only viable location for a bed and breakfast because it is located at the end of the street and Mr. Chadney would be on site to ensure the business would be run properly.

Cheryl Allen, 368 Floral Street, Astoria, confirmed that the Planning Commission had read all of her letters, including one that was submitted Friday, August 21st. She now understood why her retaining wall had been violated. The letters explain how she feels about this application. She purchased her house in 1994 and has lived there since 2003. She never had any problems with the residents from Fort George. They were respectful and parked on the street where parking was available. No one ever parked in front of her house. The pictures show there have been major parking problems in the last few months. The garbage trucks have traditionally turned around on Elsie and with the double parking, delivery drivers are unable to make it down the street. When she had her kitchen redone, Home Depot had to drive a forklift up the street because they were unable to get their truck to her house. Fire engines have never been on Floral Street at Christmas time because they are unable to get up the street. Parking is an issue. She believed Mr. Linza did an outstanding job on both houses. Originally, when he bought the house next to her it was to be used as a retirement home. Later, when he bought the second house, he said he wished he had not put so much money into the first house. The bed and breakfast idea came up within the last month. Mr. Linza had suggested she have a bed and breakfast, but her home only has two bedrooms and no parking. There is one parking space across the street, next to land leased from the City by Mr. Matthew. Her daughter was parking her car there and she knew it should not have been. Mr. Matthew called the police to her house to ask that the car be removed. She has two dedicated parking spaces in front of her house, which was built in 1918. She was very concerned about the drainage of her property. She had sent pictures to the Planning Commission and said she wrote the letters because she tends to ramble. She was totally against the bed and breakfast. There have only been a few times someone double-parked in front of her, preventing her from getting to work. She loves her house and the bed and breakfast will change the integrity of the neighborhood. All summer, a dog was barking 20 or 30 feet from her house. She does not say anything to her neighbors. When she watches her grandbaby, the baby could run into the street in the blink of an eye. Unfamiliar people coming up the street have no idea how small the street is. People that are not from the area whiz up the street. If anyone should have a bed and breakfast, it should be the man who owns the house across the street from her because he lives in Germany. She agreed with Mr. Chadney, but said most of the turn-arounds were done on Elsie directly across from her lot. This is how it has been for years. She did not understand why permits were not involved. When she told Community Development Director Cronin what was going on in her side yard, he said she needed a permit because her water lines run almost underneath her walkway. Her insurance company was looking into the situation. She did not understand how so much work could be done without permits. The bed and breakfast was not Mr. Linza's original plan, but he saved the house by doing such an awesome job at the renovation.

Commissioner Spence asked Ms. Allen to show him where Elsie is on the map. She pointed to Elsie and noted her house, her two parking spots, and the place her daughter parked. She said the garbage men have never been to that part of the street, at least since 2003 when she began living there full time. She added that if Mr. Linza had begun living in his house full time, it was only within the last two weeks. She confirmed the Commissioners had seen the pictures she submitted to show the double parking and said it has occurred all summer. She used the map to show where the garbage men turn around, noting that she can see them from her bedroom window. She woke up one morning to find her entire side yard being compromised, so she asked Mr. Linza to conduct the survey. Her house was built in 1918, so it is old. However, the side yard is not useable because it is not safe.

Interim Planner Morgan confirmed that Elsie was a City street.

Ms. Allen said she had the original plans and other documents for the house at the intersection of Elsie and Floral Streets and explained where the private property was located in relation to the right-of-way.

Ellen Levy, 420 Floral Street, Astoria, said her comments might be considered impartial. She was concerned with traffic and parking. When she visits Mr. Mackey, she must drive all the way to the end of the street, turn around at the last house, and come back down the street to park west of the house at 376 Floral. There is no other place to turn around. When one of the houses was being worked on, a kid fell off the roof and broke his leg and hip. The ambulance and fire truck were not able to get down the street because it is so tight and only has one lane. The street is on a hill, so if a driver goes off the road, they end up on Alameda. She questioned how Mr. Linza would control traffic if one of his guests wanted to bring a fishing boat on a trailer. There is no way for a boat or recreational vehicle to turn around on Floral Street. She asked what kind of vehicles would be parked on

the property, small cars or trucks with extended cabs or beds. She also wanted to know if guests would be asked in advance of their arrival what kind of vehicle they would be bringing. There is no room for errors and there is no parking. Gossip has indicated that three properties, 366, 364, and the house at the very end of Floral, are potential bed and breakfasts in the making. She asked if approving this application would set a precedent. She was concerned that her residential neighborhood would become a commercial area. Making a gravel lot would require tree removal, but the trees are the only thing holding the hill in place. She asked who would be responsible for a landslide if the trees were removed to put in a parking lot. At the end of Floral at the stop sign, there is only 1½ lanes because the people who live on the southwest side of the street park on the street. Drivers cannot see cars making a right hand turn to go up Floral. She asked who would control the extra traffic and traffic of people not familiar with the area. The City will have to change the street at the stop sign because there is no room for error, especially in the winter. If a car misses the stop sign, it will end up at the river. If the City is going to add traffic in this area, the City will have to make plans.

Cheryl Allen, 368 Floral Street, Astoria, said the hill behind the red house has slid within the last 13 years and the area is a very dangerous, known slide area.

Dennis Cranston, 403 Floral Street, Astoria, said his house was near the narrow causeway. It was built in 1966 by a fishing captain named Mr. Brooks. Floral Street was a dirt road at the time and he appreciated the street improvements done by the City since then. At one time, he had rented a 10-foot U-Haul truck and the power steering had stopped working. He used the map to show where he attempted to turn around, noting that eventually he was able to turn around with a 14-point turn. He said the picture did not show the cars parked in the area. Drivers cannot get through when cars are parked there. One of the garbage trucks does come down the street and is able to turn around because it is a smaller truck. The larger garbage trucks, UPS trucks, and FedEx trucks turn around in his driveway or in the old Chinook extension, where the City decided not to build a road because of landslides. Traffic is a problem and he was concerned about the type of people that would come in for the bed and breakfast. He recently used Airbnb to rent accommodations at Core Condo Group in San Diego, California. The trip was his son's bachelor party and 12 men were partying in the rental until 5 am. Situations like this really do occur. He was also concerned about the construction of the house. The electrical systems are probably not of commercial quality since the house is a residential property. He believed the electrical systems should be commercial, but understood this was likely part of the permitting process. He also believed he should speak on behalf of those who cannot speak. Floral Street is mostly green space and each year, three or four deer have babies and use a little path through the area. The deer are like children. When strangers come zipping down the road, even at 20 miles per hour, the deer do not have a chance. He believed the City should consider the deer as part of the tourist attractions. Everyone in the neighborhood feeds the deer and they are part of the community. He believed the wildlife should be considered.

President Pearson confirmed there were no more public comments and called for the Applicant's rebuttal.

Mr. Linza said the great thing about this meeting has been the opportunity for him to hear from each of his neighbors because he had not been able to go door to door to speak with them. He is a licensed and bonded contractor and had all new wiring installed in both houses. He upgraded the house at 364 Floral with handrails and extra lighting. He has 30 years of experience and has worked with the City of Aspen. He has provided quite a bit of off street parking. Elsie is the neighborhood's turn-around. All summer, he had asked his neighbor kindly to move her junk car. He was able to back up his 22-foot truck and 24-foot trailer in the neighborhood, but needed help to avoid hitting the car while backing up. He finally had to figure out how to have the car moved because he was so fearful of hitting it. He questioned why his neighbor parked the junk car in the public parking across the street even though she had two parking spaces in front of her house. He has spent a lot of money upgrading the houses and working on the parking issues. However, it seems as if some people are just fighting him tooth and nail. He approached Ms. Allen about his idea for a bed and breakfast three months ago and she indicated she had no problems with it as long as he took care of the parking. She has been mad since he removed a stump, which he did not understand. He was trying to work with all of his neighbors. He wants the bed and breakfast so he can control the area, not make money.

President Pearson called for closing comments of Staff.

Staff noted the Applicant had just received the survey and would like a continuance. Applicants are required to obtain permits for any City work that is to be done or a change of occupancy. The City cannot require any major

upgrades unless a property is being changed from residential to commercial. Most permits for work on residential properties are approved administratively.

President Pearson closed the public hearing and called for Commission discussion and deliberation.

Commissioner Easom confirmed the Applicant had formally requested a continuance.

President Pearson asked Staff to explain Criterion F on Page 5 of the Staff report. He also wanted to know how a road is determined to be narrow. Staff explained that Floral Street is about 18 feet wide, which is the bare minimum for a fire truck. There is no on street parking along most of the street. The Comprehensive Plan requires the site to have a layout adequate for transportation activities and suitability is partially determined by the potential impact of parking facilities on safety, traffic flow and control, and emergency vehicle movement. Therefore, Staff determined that without on street parking, it would be difficult to meet this criterion. The Transportation System Plan (TSP) defines standards for streets and Floral does not comply with the current standards. A new street would have sidewalks and facilities for emergency response, but Floral is old and narrow.

Commissioner Mitchell noted that the age of the community has resulted in many substandard streets. She asked if any entity in the community would have any kind of regulatory capacity over these expanded residential uses in neighborhoods. The Planning Commission recently reviewed a similar request to increase temporary housing and the housing study indicates this may be an issue the Planning Commission should consider. Interim Planner Morgan said there was no regulatory body tasked with overseeing vacation rentals and home stay lodging, but the industry regulates itself. For example, Airbnb imposes regulations on its clients that ensure safety, like requiring a fire extinguisher and posting emergency contact information on the refrigerator. If Astoria ever implemented a vacation rental dwelling ordinance, standards could be included.

Commissioner Mitchell understood that if parking is an issue, the neighborhood must tell the City. Director Cronin added that Astoria's zoning ordinance is very permissive and contains three classifications; home stay lodging, bed and breakfast, and inn. The bed and breakfast allows from three to seven units. These uses are allowed throughout the R-3 zone, which covers large swaths of the historic center of town. Traditionally, Astoria has been very friendly towards these uses. However, these uses can sometimes cause infrastructure and transportation problems. All of the streets above downtown, including Grand, Franklin, and Irving have bed and breakfasts, and the City does not have a lot of problem with them.

Commissioner Moore said he drove by the property on Saturday and the map does not tell the whole story. Along some parts of the road, he was uncomfortable driving through the narrow spots. Once he arrived at the property, he was unsure where he should turn around. He assumed there was a place to turn around, but being unfamiliar with the area, he did not know where it was. Elsie looked like a driveway and he did not feel comfortable turning around in someone's driveway, so he drove to the end of road hoping to find a place to turn around. However, he ended up backing into someone's driveway to turn around, which he felt bad about. The road is very crowded. It did not appear the yard was deep enough to accommodate off-street parking and he believed cars would extend into the road a little bit. Adding a few more cars could make the road impossible to navigate, especially in an emergency situation. He would support a continuance if the Applicant presented a parking solution that did not add cars to the roadway. However, he was not in favor of parking on the street, adjacent to the street, or in the driveways.

Commissioner Spence agreed with Commissioner Moore. He drove by the property on Friday and was also very uncomfortable driving through the neighborhood because it was very tight. People who are not supposed to be parking on the right-of-way still encroach on the street. He was concerned that fire engines and ambulances could not get up the street. The area is too tight and too dense to accommodate more vehicles. He believed that when residents had guests, they had no choice but to park on the street. He had to turn around in the driveway at the end of the street, but it was still tight because of the other vehicles in the driveway. He was not in favor of adding more traffic and vehicles to Floral Street.

Commissioner Easom agreed the parking was an issue and he believed Applicants should present complete parking plans that include the exact location and sizes of parking spaces. He was not concerned with the narrowness of the street because he had no problems getting through in his large vehicle. A family with kids would be going in and out during the week and all weekend long. He did not believe traffic from a bed and

breakfast would be substantially greater than if a family lived in the house. Tenants can be awful and getting the landlord to remove the tenants can be very difficult. Therefore, he was in favor of granting a continuance. Commissioner Mitchell added that she was as well.

Vice President Innes said she would not want to live in the neighborhood with more cars. However, she would like to allow the continuance.

President Pearson said the Planning Commission has reviewed many bed and breakfasts over the years. They are usually approved outright even though no one really wants to live next to a bed and breakfast. He was concerned about access from Floral to the site and was unsure he would vote to approve the request even if the applicant was able to park 10 cars in the adjacent lot. Visitors have expectations about bed and breakfasts that some Astoria's other sites provide, like a place to turn around, available parking, and access. However, he would consider a continuation.

Commissioner Fitzpatrick said he would consider a continuance, but was very concerned about the parking. He appreciated and respected the work that Mr. Linza has done to the property. He visits a friend who lives on a section of Floral that has curbs and gutters and he was thankful he did not have to look for parking at the end of Floral.

Commissioner Easom moved that the Astoria Planning Commission continue Conditional Use CU15-02 by Frank Linza to September 16, 2015 at 6:30pm; seconded by Commissioner Moore. Motion passed unanimously.

President Pearson stated he would re-open the public hearing to take public comments at the next meeting. Staff confirmed a notice would be published in the newspaper.

President Pearson called for a recess at 8:59 pm. The Planning Commission meeting reconvened at 9:06 pm.

REPORTS OF OFFICERS/COMMISSIONERS: None.

NEW BUSINESS:

ITEM 6(a): Housing Study

Director Cronin explained that the Affordable Housing Study had been a City Council goal for the last two years. The study indicates there is a shortage of market rate and affordable housing in Astoria. The Planning Commission is tasked with providing feedback about the results of the study and make any recommendations the Commission would like Staff to forward to City Council. A City Council Special Work Session has been scheduled for September 14, 2015 at 7:00 pm. Commissioners are welcome to attend the work session, where Staff will set up panel discussion with local housing experts.

Interim Planner Morgan presented the Astoria Affordable Housing Study, which was included in the Staff report. He noted the report included data that will be used to develop a detailed plan to stimulate more affordable housing. Staff responded to questions and comments from Commissioners with the following key comments:

- The number of accessory dwelling units (ADUs) built since 2004, when the Development Code was amended to allow ADUs in all of Astoria's residential zones, is still unknown. However, Staff confirmed the number of units was low and noted some units may have been built without the City's knowledge. The number of ADUs will be added to the final draft of the study.
- The data indicates the lack of housing in Astoria is worse than expected. Commissioners and Staff discussed how quickly housing is filled when it becomes available, low vacancy rates, the way parking is affected by a housing shortage, and rental rates.
- Commissioner Fitzpatrick discussed how the lack of available housing has led in part to a dramatic increase in rental rates since 2013. He believed the recession a few years ago led many families to share housing and postpone relocating. Now that the recession was over, he believed Astoria's population was increasing as people were relocating to Astoria and adult children were moving out of their parent's homes.
 - Staff confirmed Astoria's estimated population from 2014 was 9590. Data for 2015 will be released soon. Once the population is above 10,000, Astoria will be in a different regulatory category under the Department of Environmental Quality (DEQ) and the Environmental Protection Act (EPA).

- Staff and Commissioners discussed how ADUs could provide some additional housing, how the City handles homes that are marketed as short-term rentals, vacant homes, and the challenges of cleaning up derelict homes. While Astoria lacks new single-family houses, the City has received hundreds of applications for permits for renovations, restorations, and rehabilitations of existing homes.
- The Coast Guard's need for housing and the lack of available land to build housing puts additional pressure on the market. The City could annex land if their priority was to create additional housing. However, if the city does not expand its boundaries, more development could occur outside the city limits. Therefore, the City should consider subsidies or some type of incentive to attract developers to build affordable housing.
 - Small chunks of land within the City limits can be developed, but many of the available sites have issues that would prevent affordable housing from being developed. Commissioners and Staff discussed possible opportunities for building new affordable housing, including building mixed use structures like the proposed new library, a subdivision on Tongue Point, and using the Mill Pond model for a mixed income neighborhood.

Staff planned to present some possible solutions to City Council on September 16th. However, the City, the County, community development corporations, and the Housing Authority will need to make a concerted effort to resolve the housing issue in the region.

President Pearson noted the importance of solving the housing problem without filling Astoria with vacation rentals.

Commissioner Moore wanted to know what the implications would be if Astoria grew beyond a population of 10,000 and asked if it was possible to maintain the current population. Staff believed the CSO projects and wastewater treatment plant would be impacted the most.

Staff added that most of the Comprehensive Plan has been updated, so the City will have to decide how many resources to use. The next step will be a City Council work session to discuss the study on September 14th. Staff hopes Council will create a task force and formally accept the study.

ADJOURNMENT:

There being no further business, the meeting was adjourned at 9:02 pm.

APPROVED:



Community Development Director



CITY OF ASTORIA

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CONSENT CALENDAR

Item 5(d)

Waiver of Downtown Overtime Parking During the Holiday Season


(No documentation is included for this agenda item)



CITY OF ASTORIA
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COMMUNITY DEVELOPMENT

October 30, 2015

TO: MAYOR AND CITY COUNCIL
FROM:  BRETT ESTES, CITY MANAGER
SUBJECT: AMENDMENT A15-03 CONCERNING RIVERFRONT VISION PLAN
IMPLEMENTATION FOR THE NEIGHBORHOOD GREENWAY AREA

Background

In 2009 the City of Astoria adopted the Astoria Riverfront Vision Plan. The Riverfront Vision Plan describes a future vision and specific recommended implementation measures related to open space, land use, and transportation plans along the Columbia River waterfront. For purposes of the Riverfront Vision Plan, the City's riverfront was divided into four plan areas: Bridge Vista, Urban Core, Civic Greenway, and Neighborhood Greenway.

Over the past few years, the City Council adopted goals regarding implementation of the Riverfront Vision Plan. In 2012-2013, the City of Astoria requested and received a Transportation and Growth Management (TGM) Code Assistance grant to develop and write updated Comprehensive Plan language, Development Code text, and map amendments to implement policies and recommendations in the City's adopted Riverfront Vision Plan for the Civic Greenway Plan Area (Phase 1) and Bridge Vista Plan Area (Phase 2). The intent of the implementation process is not to revisit or revise recommendations from the Riverfront Vision Plan which was accepted by the Astoria City Council in 2009.

The first steps in the process to implement the Riverfront Vision Plan were adoption of code amendments for the Civic Greenway Plan Area and Bridge Vista Plan Area. The Council adopted the amendments for the Civic Greenway Area at their October 6, 2014 meeting and for the Bridge Vista Area at their June 16, 2015 meeting. The adopted materials are accessible on the City's website at www.astoria.or.us, Community Development, Projects, Riverfront Vision Plan, or can be obtained at City Hall.

With the completion of Phase 1 (Civic Greenway Area) and Phase 2 (Bridge Vista Area), staff has begun work on Phase 3 (Neighborhood Greenway Area). This Area encompasses the Alderbrook neighborhood north of Lief Erikson Drive to the pier headline between 41st Street and 54th Street.

The City received funding from the Department of Land Conservation and Development (DLCD) Coastal Management Technical Assistance Grants for the FY 2014-2015 grant cycle to assist in code writing for implementation of Phase 3 of the Astoria Riverfront Vision Plan for the Neighborhood Greenway Area (41st Street to approximately 54th Street). The grant

agreement between DLCD and the City was approved by City Council on March 2, 2015. The grant funds need to be expended and the project complete by December 31, 2015.

The Planning Commission held a Town Hall Meeting on August 20, 2015 and two public work sessions (September 1, 2015 and September 16, 2015) on the draft amendments with mailed, e-mailed, and published notification to the general public as well as anyone who expressed interest in the Riverfront Vision Plan implementation process. Updates were provided to the City Council on the progress made to date on July 27, 2015 and August 31, 2015. A presentation to the City Council on the progress made to date was held on November 2, 2015. The work sessions have been well attended and public comments have been provided.

The RVP for the Neighborhood Greenway Planning Area identified Land Use Assumptions and Objectives which state that *"It is expected that very little overwater development will occur in the Neighborhood Greenway area in the future other than small-scale improvements such as docks or piers (or improvements to the River Trail trestle) which would enhance the general public access to the river."* The objectives of the RVP for this area include:

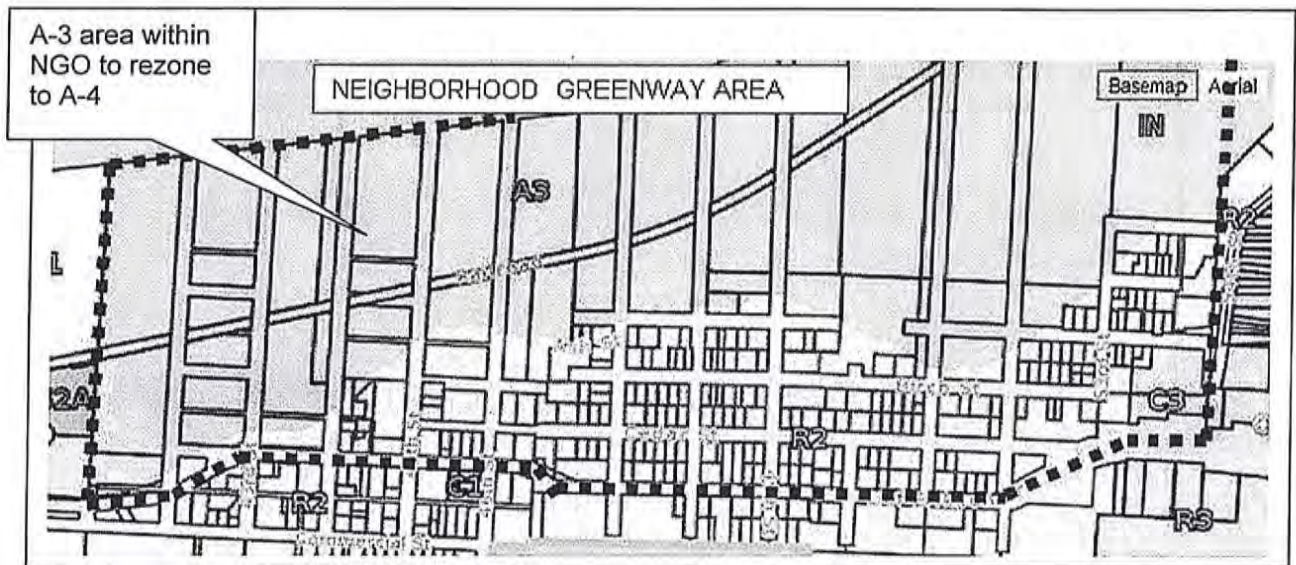
- Highlight the river's natural edge by framing views and creating path/trail systems that reflect the river's movement.
- Emphasize natural features and beauty along the edge of the river and lagoon.
- Use plantings and landscaping that provide riverbank restoration and increase habitat.
- Respect and protect the visual character of the Alderbrook Neighborhood.
- Minimize impacts of pedestrians on neighborhood residents.
- Protect river vistas to maintain physical and visual connection to the Columbia River.
- Create and maintain a sense of openness to enhance connections to the Columbia River and the Neighborhood Greenway area and improve the quality of residential areas. Open spaces should range from smaller, more intimate spaces to larger gathering places.
- Encourage maritime related uses consistent with Astoria's working riverfront such as docks, piers, marinas, and associated uses that provide jobs and maintain a historic connection to the river.

Throughout the RVP implementation process, the Planning Commission (APC) focused on these Assumptions and Objectives and did not attempt to change the Vision Plan as adopted.

Proposed map amendments will include:

1. Rezone the aquatic parcels between 41st and approximately 54th Streets, from the shoreline to the pier head line from A-3 (Aquatic Conservation) to A-4 (Aquatic Natural).

2. Apply the new Neighborhood Greenway Overlay (NGO) Zone to the Neighborhood Greenway Plan Area.



Proposed Development Code text amendments will include:

1. Add a new Neighborhood Greenway Overlay Zone to address the standards for over-water development including structure height and width, allowable uses, and landscaping, etc.
2. Add new design standards for multi-family development in the Neighborhood Greenway Plan Area.
3. Add new design guidelines for multi-family residential and non-residential development in the Neighborhood Greenway Plan Area.
4. Establish landscaping standards for multi-family residential and non-residential construction/uses. There would be no landscaping standards for single-family and two-family dwellings.
5. Allow some exemptions for the few existing over-water buildings to continue to be viable businesses thereby preserving the historic structures.
6. Limit new, over-water development to maximum height of top of bank.
7. Make miscellaneous "housekeeping" amendments related to references to the above noted amendments.

Proposed Comprehensive Plan text amendments will include:

1. Update the description of the Alderbrook Area and reference the Neighborhood Greenway Overlay Area and Riverfront Vision Plan implementation.

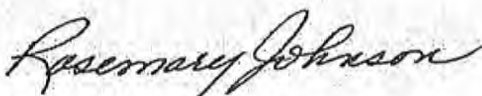
2. Acknowledge the growing impact of traffic to the neighborhood.
3. Add a policy to investigate the possibility of extending the trolley to the Alderbrook area.
4. Change designation of aquatic area from conservation to natural, and amend allowable uses in the Aquatic Natural designated areas to include the exception for existing structures.

The Planning Commission held a public hearing at the October 27, 2015 APC meetings. There were no public comments. Subsequently, the Astoria Planning Commission unanimously recommended that the City Council adopt the proposed amendments. A copy of the Staff Report and Findings of Fact as adopted by the Planning Commission is attached. Also attached to this memo are the proposed ordinances, minutes of the meetings, and public comments received. A public hearing on the Amendment has been advertised and is scheduled for the November 16, 2015 City Council meeting.

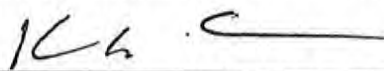
RECOMMENDATION

It is recommended that the Council hold a public hearing and consider adoption of the proposed ordinances. If the Council is in agreement with the recommendation of the Planning Commission, it would be in order for Council to hold a first reading of the two separate ordinances as follows (two separate motions and readings required):

1. Amend the Astoria Development Code Pertaining to the Neighborhood Greenway Area issues; amend the Astoria Land Use and Zoning Map to rezone an aquatic area from A-3 (Aquatic Conservation) to A-4 (Aquatic Natural); amend the Astoria Land Use and Zoning Map to apply the Neighborhood Greenway Overlay.
2. Amend the Comprehensive Plan Sections concerning the Riverfront Vision Plan implementation for the Neighborhood Greenway Area.

By: 

Rosemary Johnson, Special Projects Planner

Through: 

Kevin Cronin, Community Development Director

ORDINANCE NO. 15-_____

AN ORDINANCE AMENDING THE ASTORIA COMPREHENSIVE PLAN PERTAINING TO IMPLEMENTATION OF THE ASTORIA RIVERFRONT VISION PLAN FOR THE NEIGHBORHOOD GREENWAY PLAN AREA

THE CITY OF ASTORIA DOES ORDAIN AS FOLLOWS:

Section 1. Astoria Comprehensive Plan Section CP.080, Alderbrook Area, is deleted in its entirety and hereby amended to read as follows:

"CP.080. Alderbrook Area.

The Alderbrook Area generally lies between Lief Erikson Drive and the pier head line, and from ~~42nd~~ 41st to 54th Streets. It is an area of primarily older, single-family homes, with a few scattered duplexes. The area is topographically low, and some of it is in the 100-year floodplain. There are ~~two~~ four small parks ~~-, Alderbrook Hall and Park -one -on~~ at 4509 Lief Erikson Drive; LaPlante Park in the 4500 Block Cedar Street; Birch Street Ball Field at 49th and Birch Street; and Alderbrook Lagoon Beach at the foot of 53rd Street ~~the other on Birch Street.~~ The only commercial uses ~~are is the~~ a small business on Lief Erikson Drive, a motel at 54th and Lief Erikson Drive, and a construction business at 49th and Ash Streets. A commercial fishing facility was located on the waterfront between 49th and 50th Streets; however this facility is currently being converted into an art studio/retreat. The Area also overlaps with the Astoria Riverfront Vision Plan "Neighborhood Greenway" area which extends along the Riverfront from 41st Street to the east side of the Alderbrook neighborhood at approximately 54th Street and between Lief Erikson Drive and the pier head line of the Columbia River as depicted on the City's Zoning Map.

Alderbrook Area is the only older neighborhood directly on the waterfront, and this is discussed in the shorelands/estuary section. The area has historically been zoned Medium Density Residential (R-2). The adjacent Blue Ridge and Emerald Heights areas are zoned High Density Residential (R-3), and are primarily multi-family housing areas. The majority of structures in the Blue Ridge area were demolished prior to 2007 and the land is mostly vacant pending redevelopment. The adjacent motel is zoned General Commercial (C-3). The City limits line stops east of Blue Ridge and includes North Tongue Point industrial area and South Tongue Point, but excludes North Tongue Point Job Corps Center and the US Coast Guard station, although this area is in the City's Urban Growth Boundary.

Advantages of the Alderbrook Area are the proximity to the waterfront, the lack of through traffic (away from Lief Erikson Drive), the neighborhood hall and neighborhood character, and the availability of lower cost housing. However, the limited access into the Alderbrook Area from Lief Erikson Drive creates concerns with increased traffic to the area with no secondary vehicular outlets. Disadvantages include the flooding potential, the traffic along Lief Erikson Drive (a problem when crossing to the playground), and the distance to school."

Section 2. Astoria Comprehensive Plan Section CP.085.2, Alderbrook Area Policies, is deleted in its entirety and hereby amended to read as follows:

"The residential character of Alderbrook will be protected through the designation of the aquatic area from 41st Street to Tongue Point as ~~conservation~~natural, and by the present zoning pattern. Development in the 100-year flood area shall be subject to the requirements of the City's Flood Hazard Overlay Zone."

Section 3. Astoria Comprehensive Plan Section CP.085.3, Alderbrook Area Policies, is deleted in its entirety and hereby amended to read as follows:

"3. ~~Light industrial~~water-dependent / water-related development consistent with the ~~conservation~~natural estuary designation (such as commercial fishing operation) may be allowed so long as it does not conflict with the residential area and is consistent with the City's Riverfront Vision Plan."

Section 4. Astoria Comprehensive Plan Section CP.085.6, Alderbrook Area Policies, is deleted in its entirety and hereby amended to read as follows:

"6. The property west of the sewer lagoons is ~~presently~~-designated as a ~~potential~~-park site and is the current (2015) east terminus of the River Trail; it is zoned Institutional (IN) Medium-Density Residential (R-2). However, the site's use as a regional park raises problems of traffic generation on residential streets. The local community must be involved in any future decision regarding this area."

Section 5. Astoria Comprehensive Plan Section CP.085.8, Alderbrook Area Policies, is hereby amended by the addition to read as follows:

"8. Extension of the trolley service to the Alderbrook neighborhood should be investigated and considered."

Section 6. Astoria Comprehensive Plan Section CP.175.F, Uppertown / Alderbrook Subarea Plan, Aquatic and Shoreland Designations, is hereby deleted in its entirety and hereby amended to read as follows:

"The aquatic area between 29th and 41st Streets is designated Development to the pierhead line, except at the East End Mooring Basin where the designation corresponds to the outer boundary of the pier. East of 41st Street, the aquatic area is designated ~~Conservation~~Natural."

Section 7. Astoria Comprehensive Plan Section CP.150.B, Permitted Uses in Columbia River Estuary Aquatic and Shoreland Designations, Natural Aquatic, is hereby amended by the addition to read as follows:

"20. In pile supported buildings existing as of October 1, 2002, non-water dependent and non-water related uses."

Section 8. Effective Date. This ordinance and its amendment will be effective 30 days following its adoption and enactment by the City Council.

ORDINANCE NO. 15-_____

AN ORDINANCE AMENDING THE ASTORIA DEVELOPMENT CODE AND LAND USE AND ZONING MAP PERTAINING TO IMPLEMENTATION OF THE ASTORIA RIVERFRONT VISION PLAN IN THE NEIGHBORHOOD GREENWAY AREA

THE CITY OF ASTORIA DOES ORDAIN AS FOLLOWS:

Section 1. Section 14.130 through 14.138 pertaining to the Neighborhood Greenway Overlay Zone is added to read as follows:

“NGO: NEIGHBORHOOD GREENWAY OVERLAY ZONE

14.130. PURPOSE.

The purpose of the Neighborhood Greenway Overlay Zone is to implement the land use principles of the Astoria Riverfront Vision Plan, dated December 2009, as they pertain to the Neighborhood Greenway Plan Area. The Neighborhood Greenway Overlay (NGO) Zone is intended to protect views of and access to the Columbia River, provide for an enhance open space and landscaping, support limited water-dependent uses consistent with Astoria’s working waterfront. The NGO Zone extends from approximately 41st Street to the east side of Alderbrook neighborhood at approximately 54th Street and between Lief Erikson Drive and the pier head line of the Columbia River as depicted on the City’s Zoning Map.

14.131. APPLICABILITY AND REVIEW PROCEDURES.

The provisions of the Neighborhood Greenway Overlay Zone shall apply to all new construction or major renovation, where “major renovation” is defined as construction valued at 25% or more of the assessed value of the existing structure, unless otherwise specified by the provisions in this Ordinance.

Review of applications in the Neighborhood Greenway Overlay Zone is subject to the administrative procedures and approval of the Community Development Director established in Article 9.

A. Residential Development Exception

These standards shall not apply to single-family and two-family dwellings.

B. Residential Development

Applications for multi-family dwellings may be reviewed administratively subject to the Design Review Standards in Section 14.134 or through the public design review process subject to the Design Review Guidelines in Section 14.135.

C. Non-Residential and Mixed Use Development.

Applications shall be reviewed through the public design review process subject to the Design Review Guidelines in Section 14.135.

14.132. ALLOWABLE USES FOR OVERWATER DEVELOPMENT.

Outright and Conditional uses within the Neighborhood Greenway Overlay Zone shall be limited to allowable uses in the A-4 Zone (Aquatic Natural) with the following exceptions:

1. In pile supported buildings existing prior to October 1, 2002, non-water-dependent or non-water-related uses as follows are allowed as a conditional use:
 - a. Arts and crafts studios.
 - b. Bed and breakfast, home stay lodging, or inn.
 - c. Home occupation.
 - d. Professional and business office, personal service establishment limited to beauty and barber services and garment alterations.
 - e. Residential home.
 - f. Single-family dwelling.
 - g. Two-family dwelling.
 - h. Multi-family dwelling.
 - i. Off-street parking requirements for the above uses may be located in the upland zone adjacent to the use. The Planning Commission may impose additional landscape buffering to protect the adjacent residential uses.

14.133. STANDARDS FOR OVERWATER DEVELOPMENT.

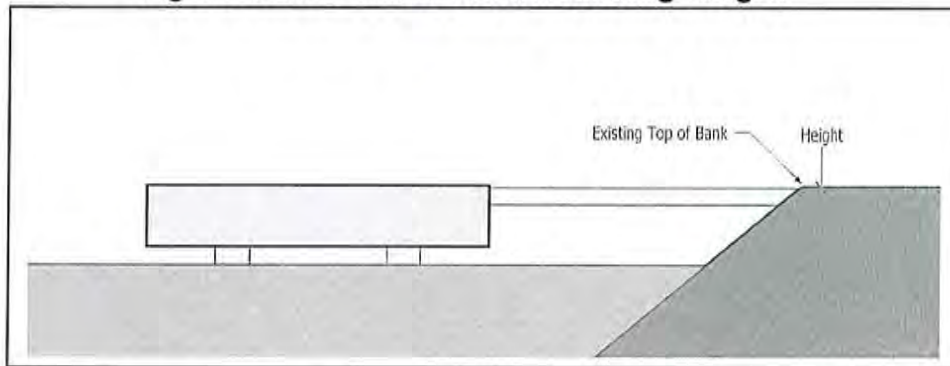
The following development standards apply to overwater development in the Neighborhood Greenway Overlay Zone. The Overwater Development standards shall also apply to on-land development north of the River Trail and/or 50' wide railroad line property between 41st Street and approximately 54th Street. In the event of a conflict between this Section and other Sections of the Astoria Development Code, this Section shall control.

Maintenance, repair, or restoration of buildings existing prior to 2002 shall be exempt from the standards of this Section. Additions and/or new construction on these buildings shall be subject to these standards.

A. Height.

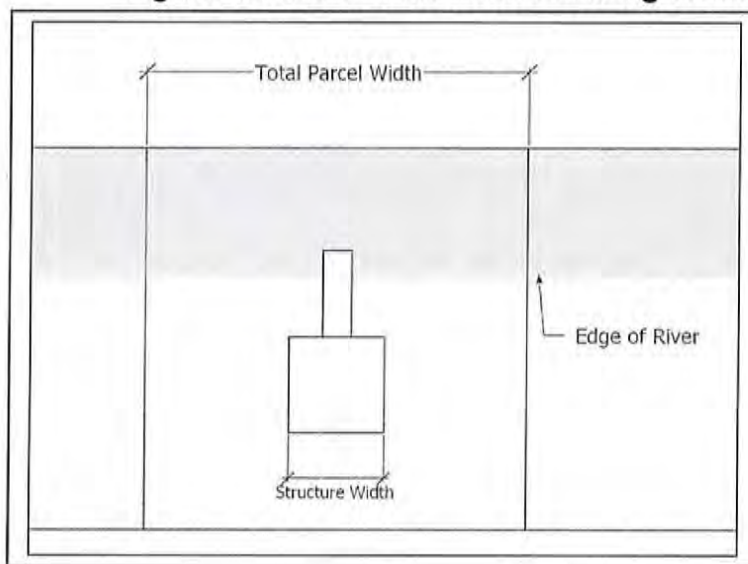
1. Maximum building height, except hand rails, shall be the top of the existing adjacent riverbank. No variance may be granted for an exception to this height limitation.

Figure 14.133-1: Maximum Building Height



- B. The maximum width of an overwater structure is 25% of the total parcel width (measured along the parcel frontage adjacent to the Columbia River) or 50 feet, whichever is smaller. In cases where total parcel width is 100 feet or less, the structure width may be up to 25 feet.

Figure 14.133-2: Maximum Building Width



14.134. RESIDENTIAL DEVELOPMENT DESIGN STANDARDS.

A. Applicability.

1. Residential.

- a. Single-family and two-family dwellings are not subject to the design standards.
- b. Multi-family dwellings proposed in the Neighborhood Greenway Overlay Zone may be reviewed in accordance with one of two review options:

- (1) Applications in compliance with Multi-Family Development Design Standards of Section 14.134 shall be reviewed pursuant to procedures for administrative review by the Community Development Director established in Article 9 unless the applicant requests review through the City's Design Review Committee pursuant to design review guidelines in Section 14.135; or
- (2) If a multi-family development applicant requests review through the City's Design Review Committee process, applications shall be reviewed in accordance with the Residential Development Design Guidelines of Section 14.135 and shall be reviewed pursuant to design review procedures in Article 9 and 14.

2. Non-Residential.

Non-residential uses may be reviewed pursuant to procedures for administrative review by the Community Development Director established in Article 9; or they may be reviewed pursuant to the Residential Development Design Guidelines of Section 14.135 and design review procedures in Article 9 and 14. The applicant shall have the option to choose between these two options.

3. Figures.

Figures included in Section 14.133 through 14.135 are included for illustrative purposes only and are not intended to be regulatory in and of themselves. If there is an inconsistency between the Figure and the Development Code text, the text shall prevail.

B. Design Standards.

1. Building Forms.

- a. All buildings shall be based on a rectangular or square form.
- b. All multi-family dwelling unit buildings shall have a front porch, at least six (6) feet deep and 60 square feet in area.

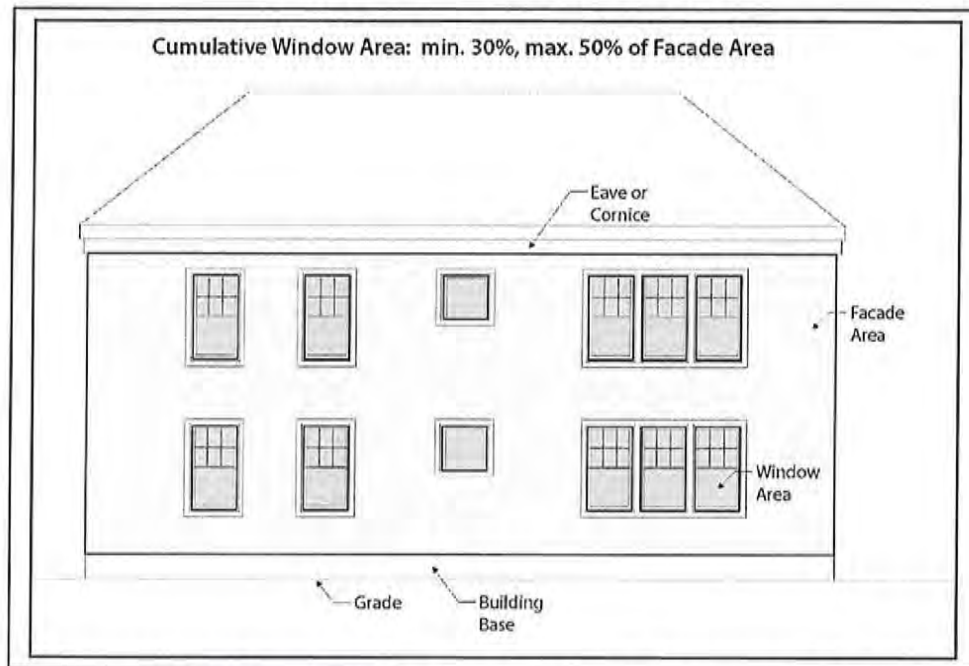
2. Window Design.

The following design standards apply to all facades.

- a. Windows required. All facades facing a right-of-way, River Trail, or common open space shall have windows.
- b. Window area. Window area shall cover a minimum of 30% of all street-

facing facade areas and shall not exceed 50% of street-facing facade areas.

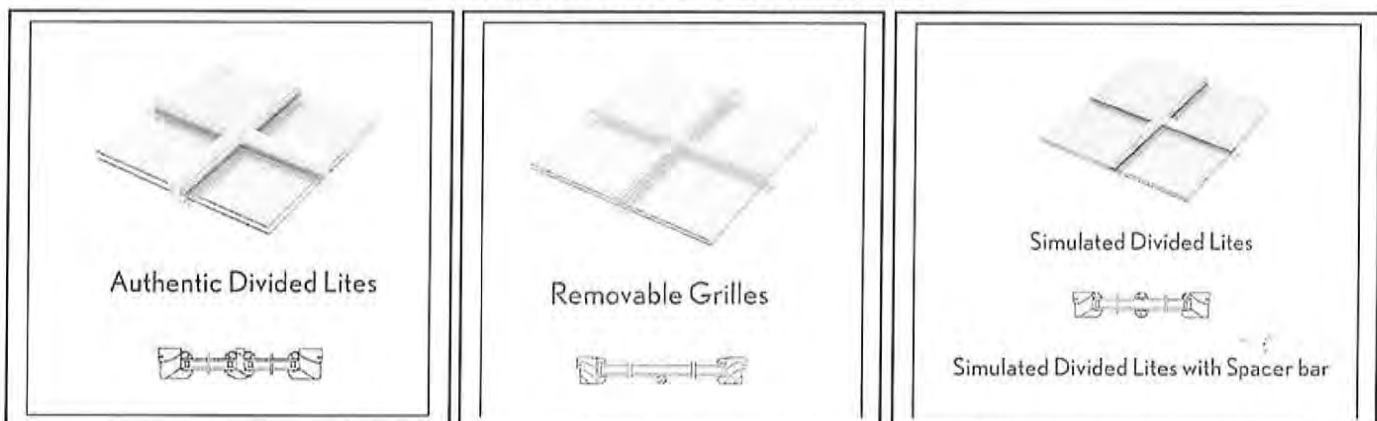
Figure 14.134-1: Window Area



c. Window lites. Window lite design shall be one of the following:

- 1) Single-lite windows; or
- 2) Multiple-lite true-divided windows; or
- 3) Combination of single and multiple-lite true-divided windows; or
- 4) Applied muntins with profile facing window exterior.

Figure 14.134-2: Window Lites

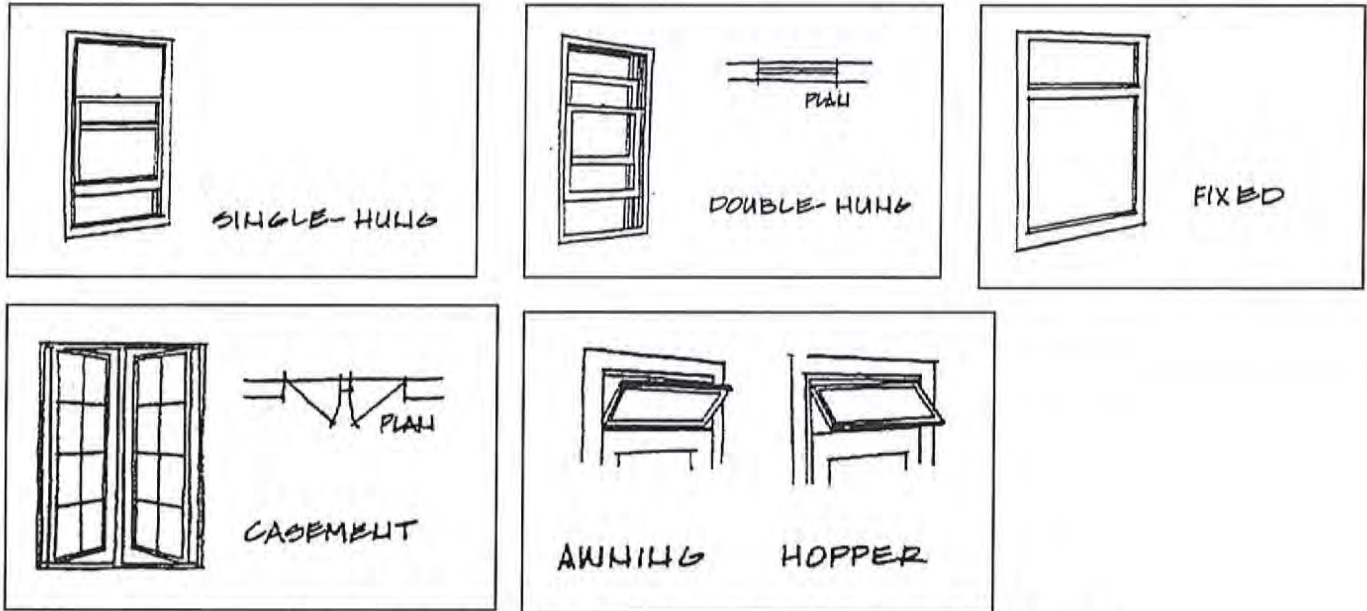


d. Windows shall be fixed or open in one of the following configurations:

- 1) Fixed window; or
- 2) Single-hung windows; or

- 3) Double-hung windows; or
- 4) Awning or hopper windows; or
- 5) Casement windows.

Figure 14.134-3: Fixed and Opening Windows

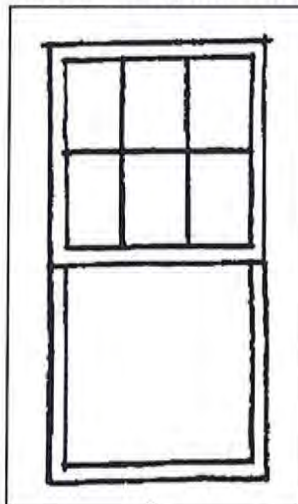


e. Window shape. Window shape shall be one of the following:

- 1) Vertical rectangle; or
- 2) Square.
- 3) Arched or decorative windows are permitted but should not exceed more than 30% of the total window coverage on all facades of the building.

Figure 14.134-4: Window Shapes

Vertical rectangular window

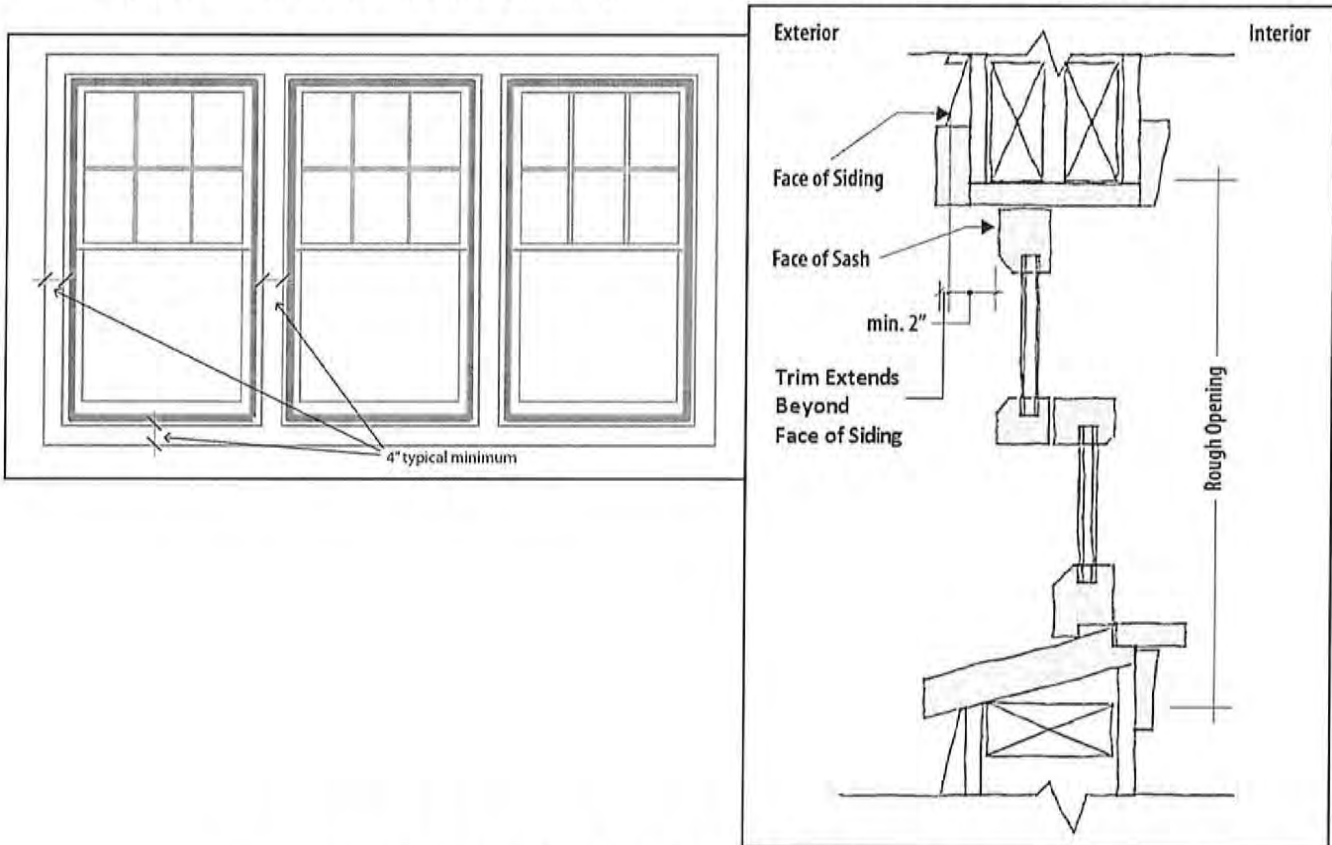


Examples of arched or decorative windows



- f. Window detailing. Windows shall have casings/trim, sills, and crown moldings. Window detailing shall meet the following requirements.
- 1) Casings/trim shall have minimum dimensions of 5/4 inch x 4 inch and shall extend beyond the facade siding.
 - 2) Windows shall be recessed a minimum distance of two (2) inches from the trim surface to ensure a shadow line/effect.

Figure 14.134-5: Window Detailing – Trim and casement location and dimensions

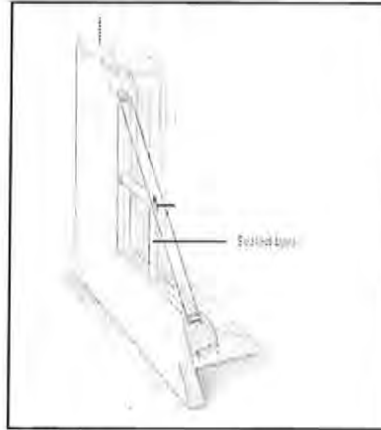


- g. Window design prohibited. The follow window design features are prohibited.

- 1) Applied muntins that have no profile.
- 2) Smoked, tinted, or frosted glass, except for bathroom windows not on the street-facing facade.
- 3) Mirrored glass.

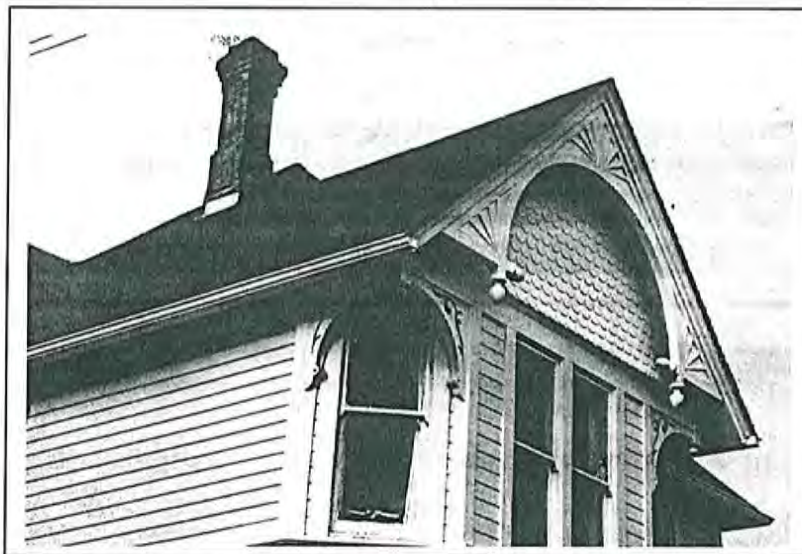
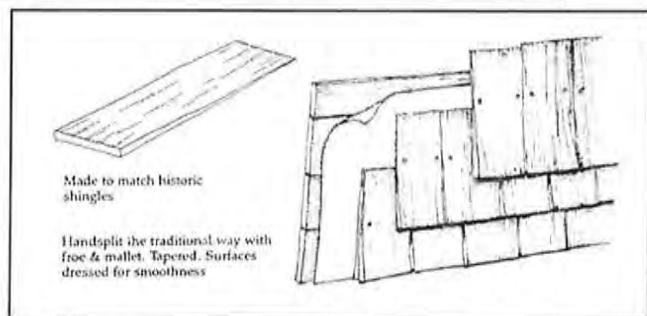
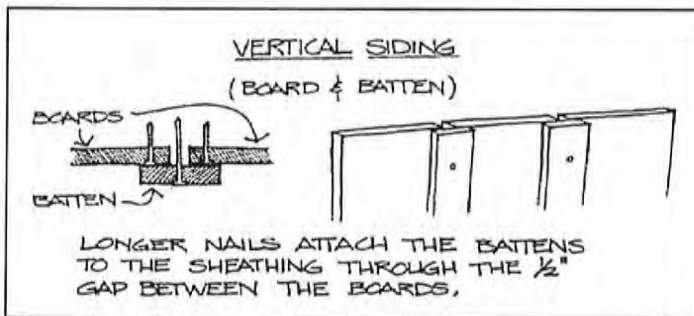
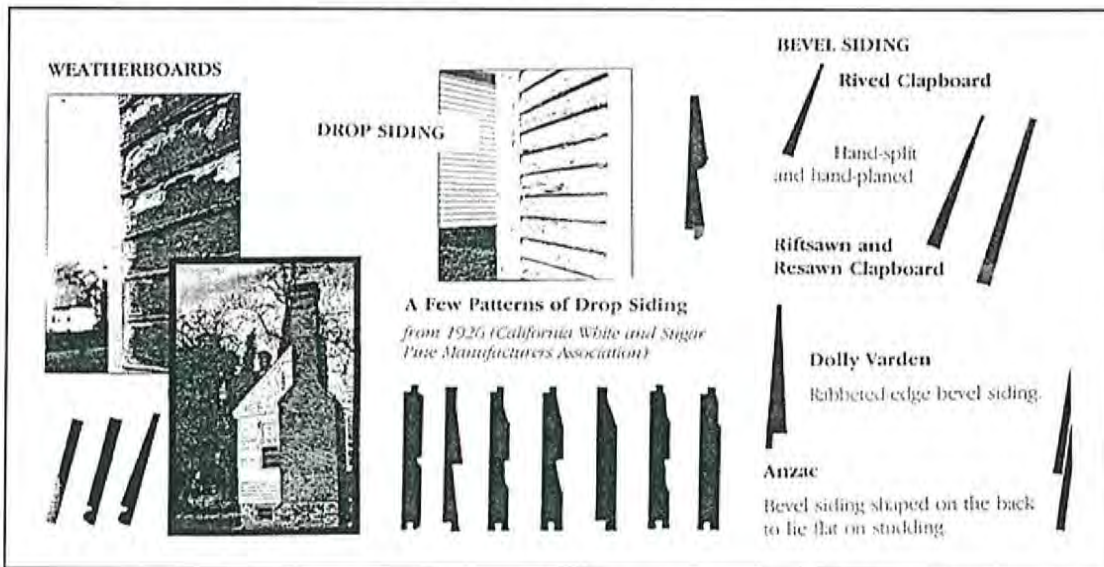
Figure 14.134-6: Window Design Prohibited

Muntins with no profile



3. Exterior Wall Treatments and Materials.
 - a. A minimum of 80% of exterior walls shall be constructed of one or more of the following sets of treatments and materials.
 - 1) Drop siding; or
 - 2) Weatherboard siding; or
 - 3) Clapboard; or
 - 4) Rectangular wood shingle or shake; or
 - 5) Decorative wood shingle or shake; or
 - 6) Board and batten.
 - b. Horizontal siding shall have six inches or less exposure.
 - c. Vertical board and batten shall have true battens.

Figure 14.134-7: Exterior Walls – Permitted Materials



- d. Paneled material shall be applied in a manner which avoids the occurrence of seams along the wall plane. Where seams cannot be avoided, they shall be located in a manner that relates logically to windows and other architectural features of the facade. Horizontal seams shall be covered by a trim board or cornice piece.

Figure 14.134-8: Exterior Walls – Seam Treatment

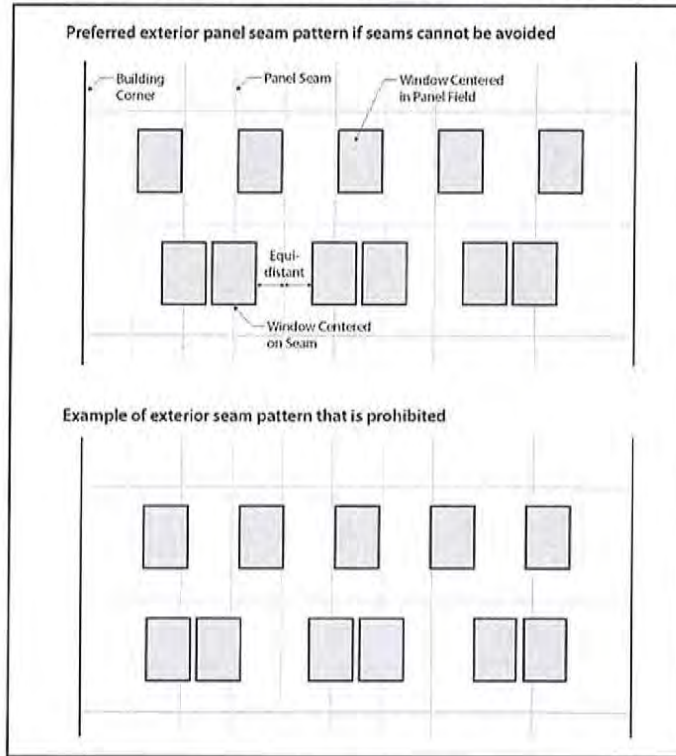
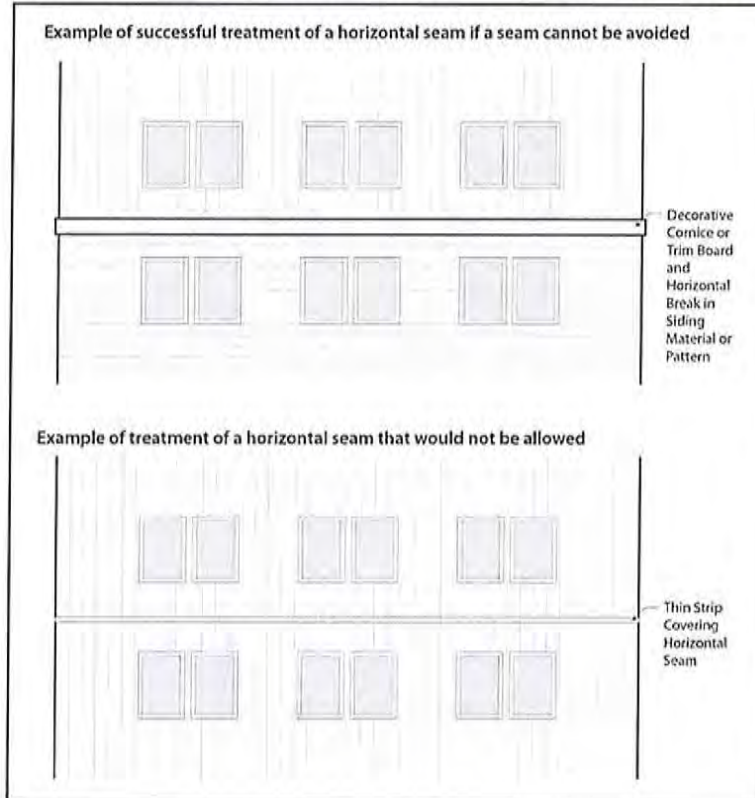


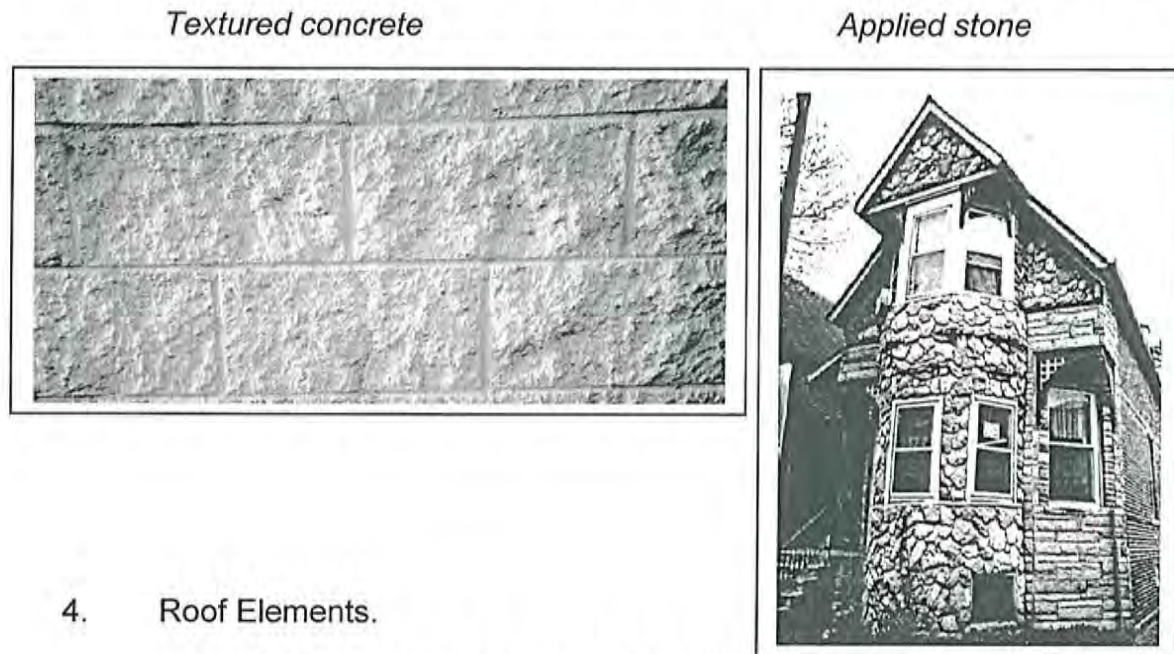
Figure 14.134-9: Exterior Walls – Horizontal Seam Treatment



e. Exterior wall treatments and materials prohibited. The following types of treatments and materials are prohibited.

- 1) Exposed textured concrete block.
- 2) Flagstone or other applied stone products.
- 3) Precast concrete or decorative concrete panels.
- 4) Plywood paneling.

Figure 14.134-10: Exterior Wall Treatments and Materials Prohibited



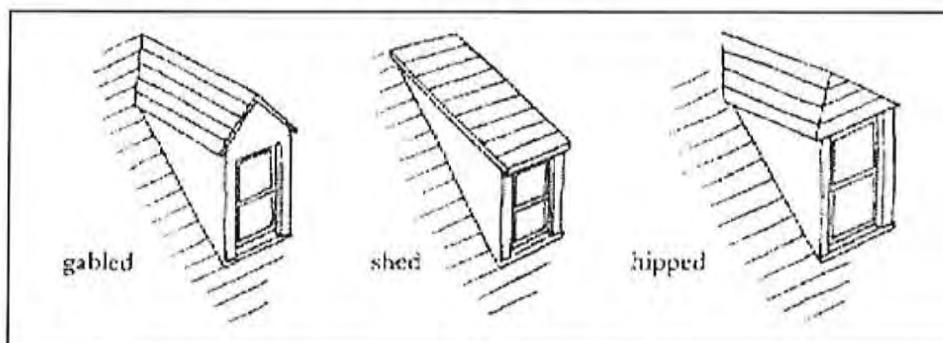
4. Roof Elements.

a. Roof elements permitted. The following roof design elements are permitted.

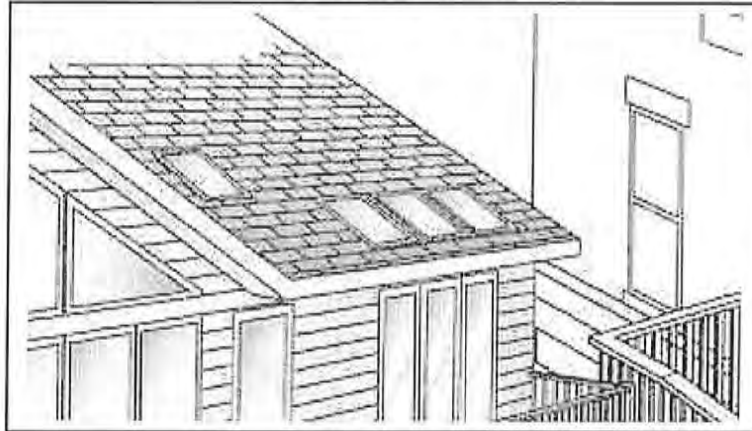
- 1) Dormers with gable, hip, or shed roofs.
- 2) Flat panel skylights or roof windows on secondary elevations.

Figure 14.134-11: Roof Elements Permitted

Gabled, shed, and hipped dormers



Flat panel skylights



- b. Roof elements prohibited. The following roof design elements are prohibited.
- 1) False mansard or other applied forms.
 - 2) Dome skylights.

Figure 14.134-12: Roof Elements Prohibited

False mansard roof



6. Signs.

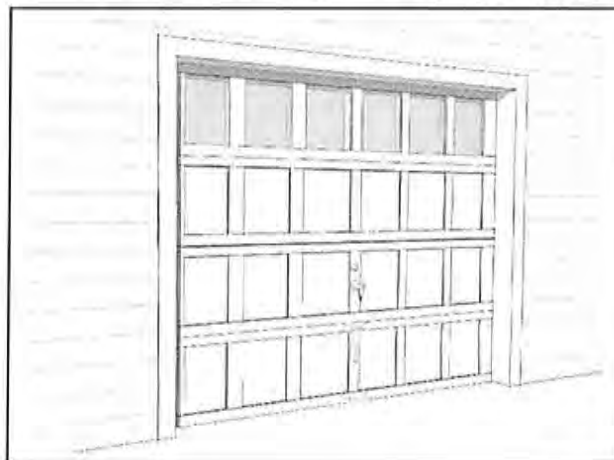
Signs are subject to the sign provisions in Section 8.040 and 8.160.

7. Doors.
 - a. Doors shall have at least one lite (glass) panel.
 - b. Sliding doors are not permitted on the ground floor of the front facade.
 - c. All materials are permitted.
 - d. Metal or metal-clad doors shall be painted.
8. Garage Doors.

The following design standards apply to attached and detached garages:

- a. A minimum of 10% of each garage door shall be window panels, raised trim, or other architectural details.

Figure 14.134-13: Garage Doors Permitted



14.135. DESIGN GUIDELINES.

A. Purpose.

These guidelines promote architectural elements that unify the Neighborhood Greenway Area by encouraging styles characteristic of Astoria. The historic architecture of Astoria is represented by a variety of styles. Differences in details may be seen from one neighborhood to the next. These guidelines advocate the simplicity of design which is characteristic of Alderbrook and the working man's neighborhood. Building styles and details not inspired by Astoria's past will be discouraged but not prohibited. Monotony of design should be avoided. Variety of detail, form, and siting should be used to provide visual interest.

The Guidelines make reference to, but do not require the use of, historic materials. Contemporary substitutions (i.e. composite materials), will not be discouraged if their

texture, profile, and proportions are similar to those materials with historic precedent. As guidelines, the code provisions shall encourage or allow for design features or approaches that provide flexibility and discretion for the appropriate review body to interpret and apply the guidelines.

B. Design Review Process.

All uses proposed in the Neighborhood Greenway Overlay Zone other than residential single-family and two-family dwellings, or public utilities that are not located within an enclosed structure shall be reviewed in accordance with the design review guidelines of Section 14.135 pursuant to the design review procedures in Article 14.

C. Design Guidelines.

The following design guidelines shall apply to multi-family dwelling development that does not comply with the design standards of Section 14.134 and/or all non-residential development except public utilities that are not located within an enclosed structure.

1. Building Forms.

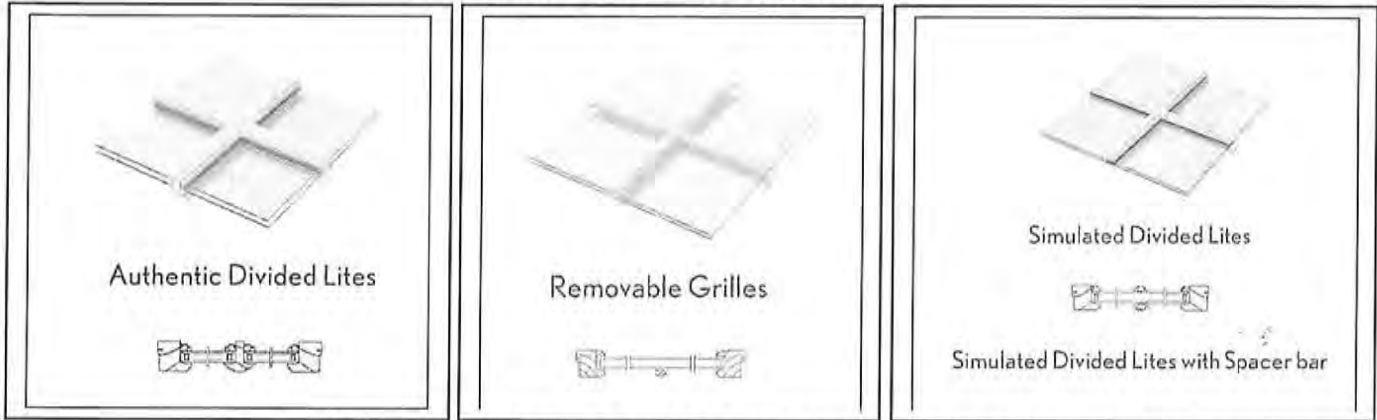
- a. All buildings should be based on a rectangular or square form.
- b. All multi-family dwelling unit buildings should have a front porch, at least six (6) feet deep and 60 square feet in area.

2. Window Design.

The following design guidelines apply to all facades.

- a. Windows required. All facades facing a right-of-way, River Trail, or common open space should have windows.
- b. Window area. Window area should be included on each level or story of the facade.
- c. Window lites. Window lite design should be one of the following:
 - 1) Single-lite windows; or
 - 2) Multiple-lite true-divided windows; or
 - 3) Combination of single and multiple-lite true-divided windows; or
 - 4) Applied muntins with profile facing window exterior.

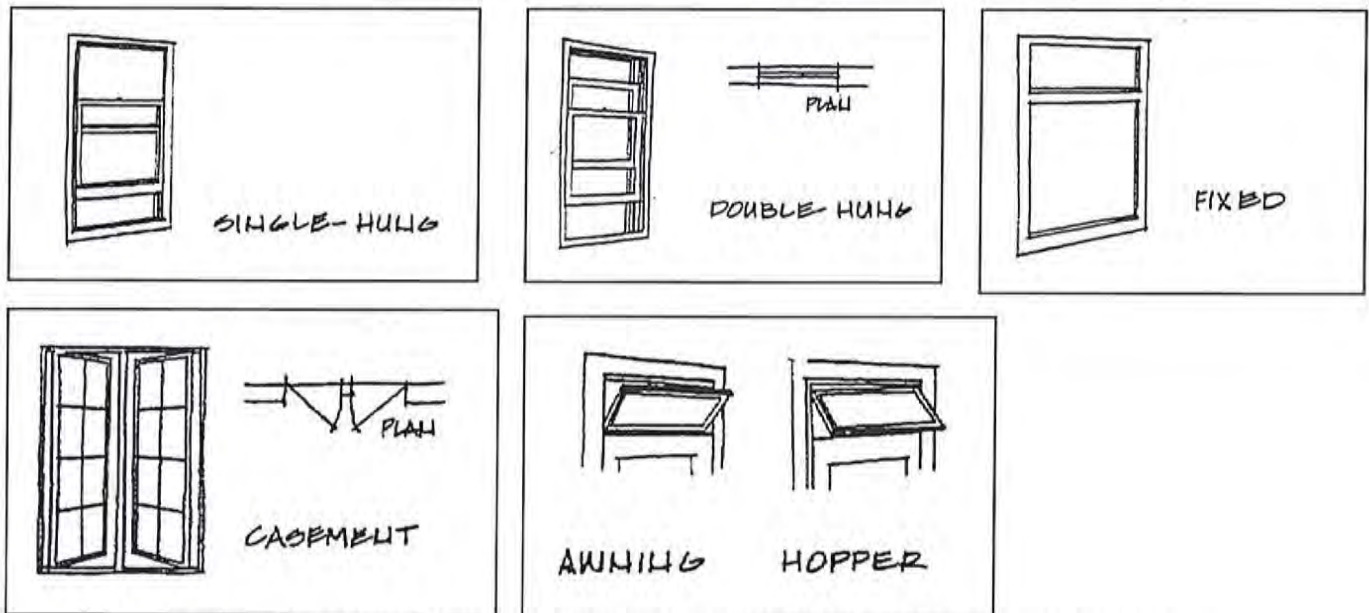
Figure 14.135-1: Window Lites



d. Windows should be fixed or open in one of the following configurations:

- 1) Fixed window; or
- 2) Single-hung windows; or
- 3) Double-hung windows; or
- 4) Awning or hopper windows; or
- 5) Casement windows.
- 6) Sliding windows may be used on non-residential structures in commercially zoned properties.

Figure 14.134-2: Fixed and Opening Windows



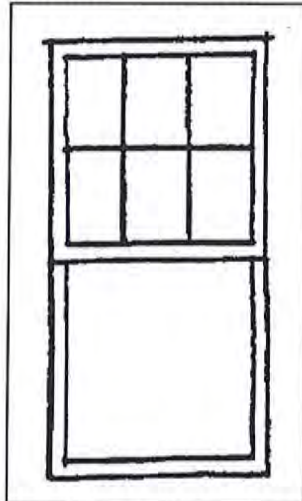
e. Window shape. Window shape should be one of the following:

- 1) Vertical rectangle; or
- 2) Square.
- 3) Arched or decorative windows are permitted but should make up a

relatively small percentage of all windows in the structure.

Figure 14.135-3: Window Shapes

Vertical rectangular window



Examples of arched or decorative windows



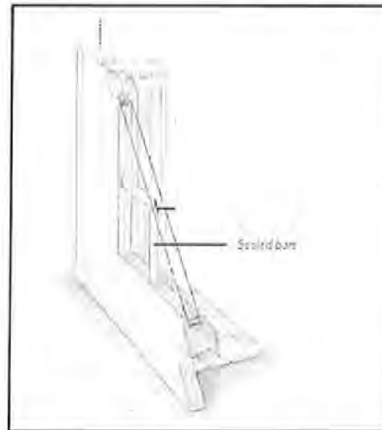
- f. Window detailing. Windows should have casings/trim, sills, and crown moldings. Casings and sills shall create a clear visible distinction and change in depth between the windows and surrounding facade.

- g. Window design discouraged. The follow window design features are discouraged.
 - 1) Applied muntins that have no profile.
 - 2) Smoked, tinted, or frosted glass, except for bathroom windows not on the street-facing facade.
 - 3) Mirrored glass.

†

Figure 14.135-4: Window Design Discouraged

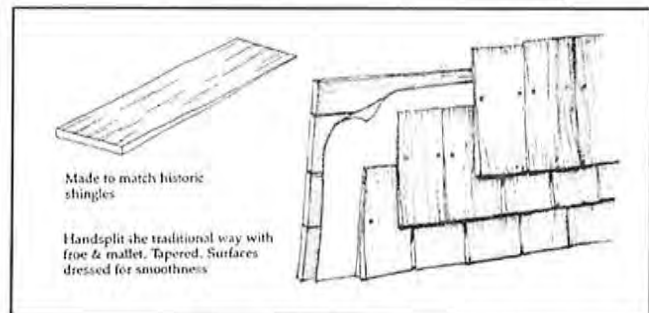
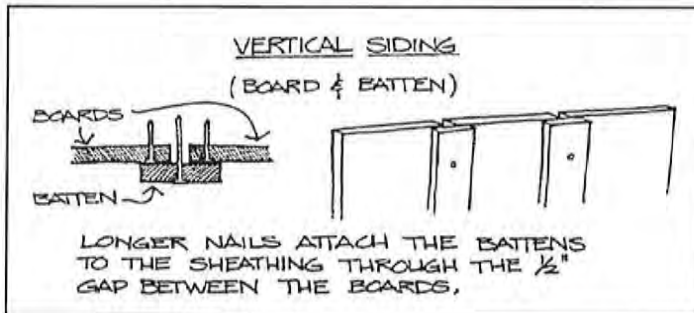
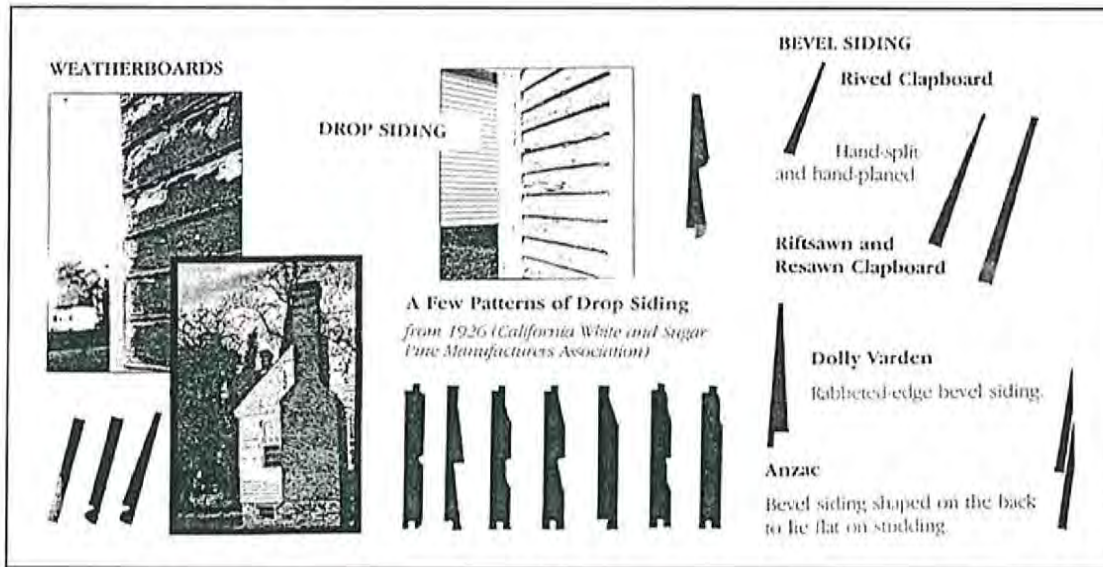
Muntins with no profile



3. Exterior Wall Treatments and Materials.

- a. Exterior walls should be constructed of one or more of the following sets of treatments and materials.
 - 1) Drop siding; or
 - 2) Weatherboard siding; or
 - 3) Clapboard; or
 - 4) Rectangular wood shingle or shake; or
 - 5) Decorative wood shingle or shake; or
 - 6) Board and batten.
 - 7) Fiber cement siding in the treatments noted above may be used but shall have a smooth, not textured, finish.
 - 8) Stucco may be used on non-residential structures in commercially zoned properties.
 - 9) Precast concrete or decorative concrete panels may be used on non-residential structures in commercially zoned properties.
- b. Horizontal siding should have six inches or less exposure.
- c. Vertical board and batten should have true battens.

Figure 14.135-5: Exterior Walls – Encouraged Materials



e. Exterior wall treatments and materials discouraged. The following types of treatments and materials are discouraged.

- 1) Exposed textured concrete block.
- 2) Flagstone or other applied stone products.
- 3) Precast concrete or decorative concrete panels, except on non-residential structures in commercially zoned properties.
- 4) Plywood paneling.

Figure 14.135-6: Exterior Wall Treatments and Materials Discouraged

Textured concrete



Applied stone

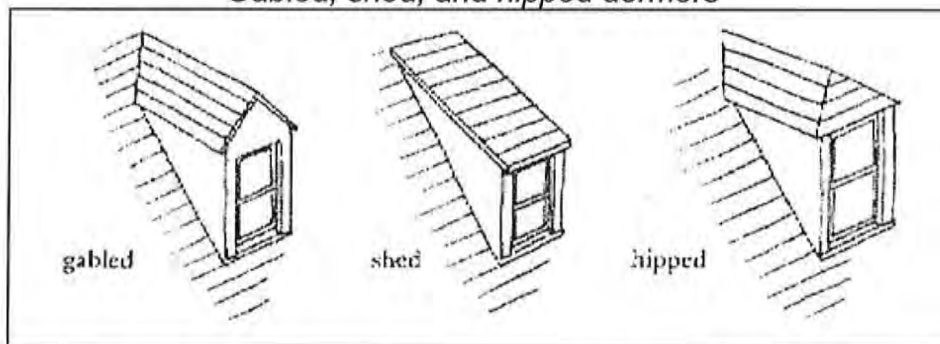


4. Roof Elements.

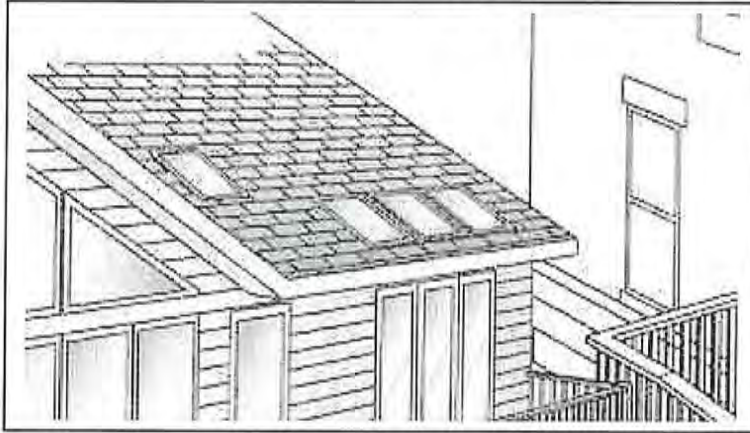
- a. Roof elements encouraged. The following roof design elements are encouraged.
 - 1) Dormers with gable, hip, or shed roofs.
 - 2) Flat panel skylights or roof windows on secondary elevations.

Figure 14.135-7: Roof Elements Encouraged

Gabled, shed, and hipped dormers



Flat panel skylights



b. Roof elements discouraged. The following roof design elements are discouraged.

- 1) False mansard or other applied forms.
- 2) Dome skylights.

Figure 14.135-8: Roof Elements Discouraged

False mansard roof



6. Signs.

Signs are subject to the sign provisions in Development Code Article 8.

7. Doors.

- a. Doors should have at least one lite (glass) panel except on non-residential structures in commercially zoned properties.
- b. Sliding doors are discouraged on the ground floor of the front facade except on non-residential structures in commercially zoned properties.
- c. All materials are permitted.
- d. Metal or metal-clad doors should be painted.

14.137. OTHER DEVELOPMENT STANDARDS.

A. The following development standards are applicable within the Neighborhood Greenway Overlay Zone.

1. Exterior lighting.

Outdoor lighting shall be designed and placed so as not to cast glare into adjacent properties. Light fixtures shall be designed to direct light downward and minimize the amount of light directed upward. The Community Development Director may require the shielding or removal of such lighting where it is determined that existing lighting is adversely affecting adjacent properties or contributing to light directed into the night sky.

2. Fences.

Fences located between the River Trail and the Columbia River shall not exceed a height of three (3) feet.

14.138. LANDSCAPING.

Landscaping is required in the Neighborhood Greenway Overlay Zone in accordance with the provisions in this Section and those in Section 3.120 to 3.125. The provisions in this Section apply to new construction or exterior renovations with a value of at least 20% of the assessed value of the structure, or in the event of installation of new parking areas for multi-family dwellings and/or development other than single-family or two-family dwellings. Single-family and two-family dwellings are not subject to the landscaping standards except as noted.

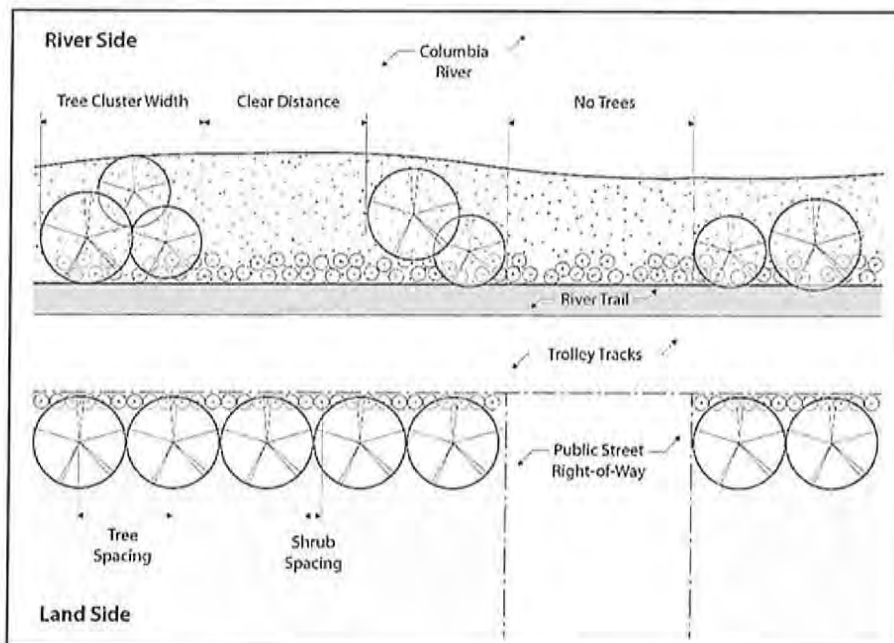
A. River Side or Riparian Standards.

1. Height and Spacing.

- a. Maximum shrub height is 30 inches.
- b. Maximum width of clusters of trees is 30 feet.

- c. Clusters of trees shall have a minimum of 50 feet clear between branches at maturity.
- d. Trees are not permitted to be planted on the river side of the River Trail within the extended public right-of-way or view corridor extending from it for a distance of 70 feet centered on the right-of-way centerline.
- e. Trees shall not exceed 25 feet in height at maturity
- f. Maximum height of fences is three (3) feet.

Figure 14.138-1: River Side/Riparian Landscaping



2. Native Plants.

See Section 3.125 concerning use of native plants and list of recommended native plants.

3. Landscaping Credits for Non-Vegetation Features.

- a. The Community Development Director may approve non-vegetative features to account for up to 40% of required landscaping when the features consist of hardscaped pedestrian-oriented areas (e.g., courtyards, plazas). Permeable paving and other stormwater management techniques are encouraged in the design of these areas.

- b. An application proposing more than 40% of required landscaping be credited by non-vegetative features is subject to approval in accordance with procedures in Article 9 and Article 12.
 - c. Non-vegetative features allowed in the public right-of-way and/or on the River Trail in lieu of required landscaping shall be maintained by the applicant. There shall be a maintenance agreement or other City approved agreement. Failure to maintain or loss of the non-vegetative feature will result in the requirement for installation of the landscaping in accordance with the Code at the time of the loss.
2. Parking Area Landscaping.
- a. Landscaping required between parking areas, streets, and sidewalks in accordance with Section 3.120.A.7 shall also be required between parking areas and the River Trail.
 - b. Landscaping shall minimize pedestrian exposure to parking lots with a hedge or a decorative fence that is 36" to 42" high.
 - c. Maximum tree height and width in parking areas shall be 15 feet at maturity.
3. Landscaping Credits for Non-Vegetation Features.
- a. The Community Development Director may approve non-vegetative features to account for up to 25% of required landscaping when the features consist of the following:
 - (1) Hardscaped pedestrian-oriented areas (e.g., courtyards, plazas); and/or
 - (2) At least one of the following amenities meeting the City approved design within the public right-of-way and/or River Trail right-of-way:
 - (a) bike rack
 - (b) bench
 - (c) table
 - (d) drinking fountain
 - (e) directional or interpretive/information signage
 - (f) trash or recycling container
 - (g) lighting
 - (h) restroom

Permeable paving and other stormwater management techniques are encouraged in the design of these areas.

exceeding 80% of its fair market value as indicated in Section 3.190.D, provided the reconstruction of the building complies with the following standards:

1. The building shall be rebuilt on the same location on the lot, or in compliance with the setback standards for the underlying zone. This does not allow any construction beyond the property lines; and
2. The square footage of the replacement structure and/or replacement uses does not exceed the square footage of the original structure and use classifications by more than 10%; and
3. The height of the building shall be the same or less than the existing building height prior to destruction; and
4. If the property is within an area subject to architectural design review standards, the design of the replacement structure shall comply with those architectural standards; and
5. Substantial construction of the building shall begin within two years of the date of destruction, unless an extension has been granted in accordance with Section 9.100; and
6. All other City and Building Codes relative to construction, including but not limited to, geologic concerns, stormwater management, grading, driveways, sidewalks, etc. shall apply.”

Section 4. Section 3.180.D pertaining to Nonconforming Uses is added to read as follows:

“D. Reestablishment of Existing Non-Conforming Uses in Overwater Buildings

Nonconforming uses in overwater buildings located between 16th and 41st Street within the Civic Greenway Overlay Area existing prior to 2013, and between 41st and approximately 54th Street within the Neighborhood Greenway Overlay Area existing prior to 2015 may be reestablished if the building housing the use is unintentionally destroyed by any means to an extent exceeding 80% of its fair market value as indicated in Section 3.190.D, provided the reconstruction of the building complies with the standards in Section 3.190.F and reestablishment of the use occurs within one year of the completion of construction. Completion of construction shall be determined by issuance of a temporary and/or final Certificate of Occupancy from the Building Official.”

Section 5. The 1992 Astoria Land Use and Zoning Map is amended to apply the Neighborhood Greenway Overlay (NGO) Zone as indicated on the map shown as Attachment A and further described below:

The area is generally described as 41st Street to 54th Street north of Lief Erikson Drive to the pier head line.

Section 6. The 1992 Astoria Land Use and Zoning Map is amended to rezone the following area from A-3 (Aquatic Conservation) to A-4 (Aquatic Natural) as indicated on the map shown as Attachment B and further described below:

The water area generally described as 41st Street to approximately 53rd Street north of the shoreline to the pier head line.

Section 7. Section 9.100.B, Permit Extensions, is amended to read as follows:

“Permit extension may be granted for all land use permits. Extensions may also be granted for time limits applicable to non-conforming buildings and/or non-conforming uses located over water between 16th and 41st approximately 54th Streets as described in Sections 3.180.D and 3.190.F. One year extensions may be granted in accordance with the requirements of this Section as follows.”

Section 8. Astoria Development Code Section 14.015.A, Gateway Overlay Zone, General Provisions, map exhibit only is deleted in its entirety.

Section 9. Astoria Development Code Section 14.010, Definitions, is renumbered as follows:

“14.001. DEFINITION.

As used in Article 14, unless the context requires otherwise, the following words shall have the meaning indicated:

“SHOULD: A requirement, unless it can be shown that to comply with the requirement would be unreasonable, impractical, or unfeasible. Economic hardship alone shall not be justification for noncompliance with the requirement, but may be considered in conjunction with other reasons for noncompliance.”

“ADJACENT: Any lot abutting the subject parcel (including all tax lots included as part of the subject parcel development) excluding rights-of-way.”

Section 10. Effective Date. This ordinance and its amendment will be effective 30 days following its adoption and enactment by the City Council.

ADOPTED BY THE COMMON COUNCIL THIS ____ DAY OF _____, 2015.

APPROVED BY THE MAYOR THIS ____ DAY OF _____, 2015.

Mayor

ATTEST:

Brett Estes, City Manager

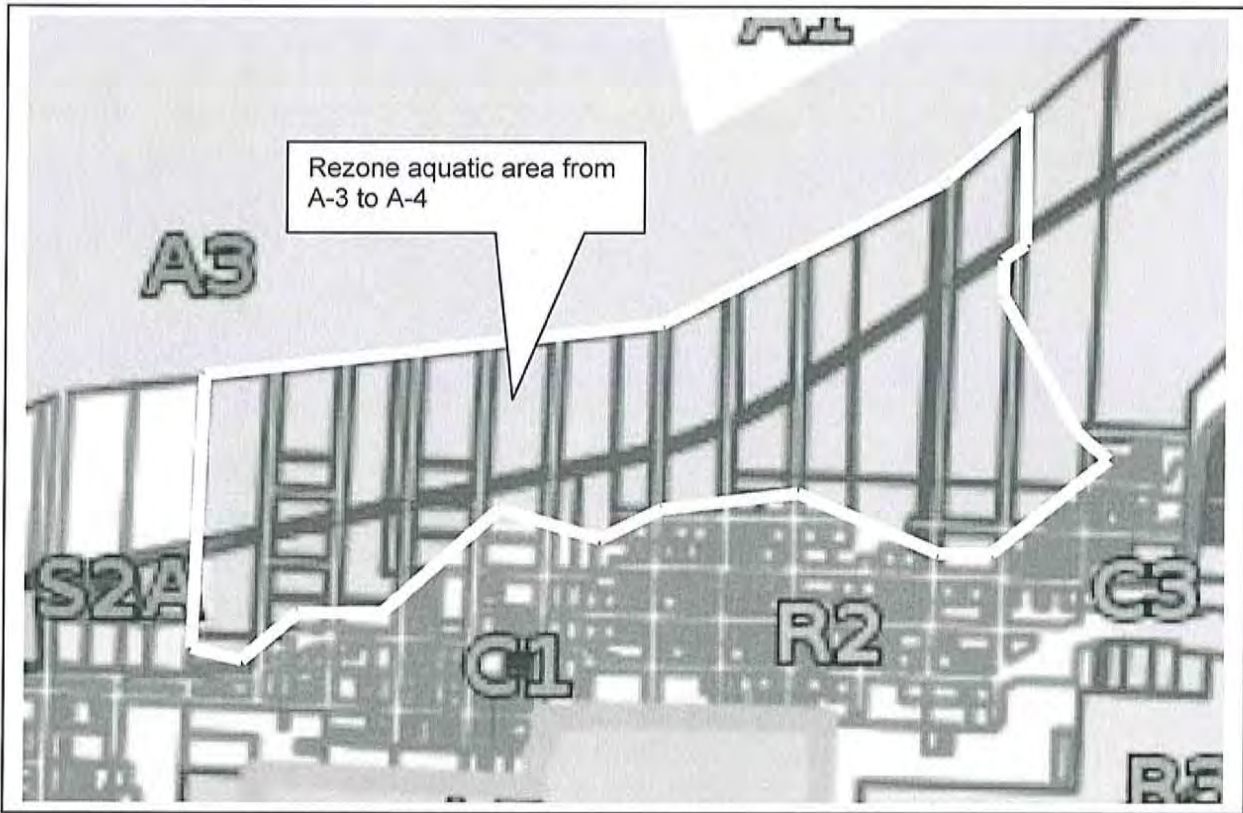
ROLL CALL ON ADOPTION: YEA NAY ABSENT

Commissioner Nemlowill
 Herzig
 Price
 Warr

Mayor LaMear

Ordinance 15-____ - Attachment B

A-3 (Aquatic Conservation) to A-4 (Aquatic Natural) Zone



BEFORE THE ASTORIA PLANNING COMMISSION
OF THE CITY OF ASTORIA

IN THE MATTER OF A AMENDMENT)

FOR THE FOLLOWING PROPERTY: MAP T8N-R9W)
SECTION 2, 3D, 9AA, 10, 10AB, 10AD, 10BA, 10BB, 10BD;)
NEIGHBORHOOD GREENWAY AREA 41ST STREET TO)
APPROXIMATELY 54TH STREET & LIEF ERIKSON DRIVE)
TO THE PIER HEAD LINE, ASTORIA OR 97103)

ZONING: A-3, AQUATIC CONSERVATION;)
R-2, MEDIUM DENSITY RESIDENTIAL;)
C-3, GENERAL COMMERCIAL; IN, INSTITUTION)

APPLICANT: COMMUNITY DEVELOPMENT DIRECTOR,)
CITY OF ASTORIA, 1095 DUANE, ASTORIA OR 97103)

ORDER NO. A15-03

The above named applicant applied to the City for Amendment A15-03 to amend the Development Code, Comprehensive Plan, and Astoria Land Use & Zoning Map to implement the Riverfront Vision Plan in the Neighborhood Greenway Area, generally described as between 41st Street and approximately 54th Street, Lief Erikson Drive to the pier head line, Astoria, Oregon 97103.

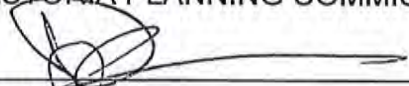
A public hearing on the above entitled matter was held before the Astoria Planning Commission on October 27, 2015; and the Astoria Planning Commission closed the public hearing and rendered a decision at the October 27, 2015 meeting.

The Planning Commission found the proposed amendment to be necessary and recommends to the Astoria City Council that the proposed amendment be approved. *A copy of the application, all documents and evidence relied upon by the applicant, the staff report, and applicable criteria are available for inspection at no cost and will be provided at reasonable cost.*

DATE SIGNED: OCTOBER 27, 2015

DATE MAILED: 10-28-15


ASTORIA PLANNING COMMISSION



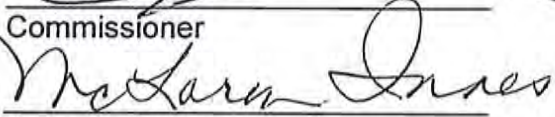
President



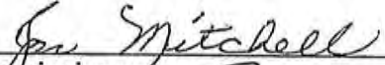
Commissioner



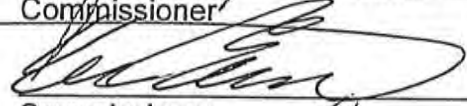
Commissioner



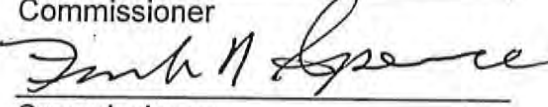
Commissioner



Commissioner



Commissioner



Commissioner



CITY OF ASTORIA
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COMMUNITY DEVELOPMENT

October 12, 2015

TO: ASTORIA PLANNING COMMISSION
FROM: ROSEMARY JOHNSON, SPECIAL PROJECTS PLANNER
SUBJECT: AMENDMENT REQUEST (A15-03) ON RIVERFRONT VISION PLAN
IMPLEMENTATION ORDINANCE FOR NEIGHBORHOOD GREENWAY AREA

I. BACKGROUND SUMMARY

- A. Applicant: Community Development Department
City of Astoria
1095 Duane Street
Astoria OR 97103
- B. Request: Amend the Development Code and Zoning map to implement the Riverfront Vision Plan in the Neighborhood Greenway (41st to approximately 54th Street, Lief Erikson Drive to the Columbia River Pierhead Line); add Neighborhood Greenway Overlay zone; add design standards for multi-family development; add design guidelines for residential and non-residential development; miscellaneous related changes with new code references in Sections 1.300, 3.180.D, 3.190.F, 9.100.B, 14.010; and rezone the existing A-3 (Aquatic Conservation) to A-4 (Aquatic Natural) zone. Amend the Comprehensive Plan Sections CP.080 & CP.085, Alderbrook Area; CP.175.F, Columbia River Estuary Aquatic and Shoreland Designations; add CP.150.B.20, Aquatic Natural Conditional Uses. Add Neighborhood Greenway Overlay Zone to Land Use and Zoning Map.
- C. Location: City-wide within the Neighborhood Greenway Plan Area

II. BACKGROUND

In 2008-2009, the City of Astoria developed the Riverfront Vision Plan (RVP) to address issues dealing with open space, land use, and transportation along the Columbia River. Significant public involvement opportunities were designed to gain public input. This process was initiated to plan for these issues in a comprehensive manner and to set a framework for the future of the study area. The City's north Riverfront (Columbia River to West Marine / Marine Drive / Lief Erikson Drive) was divided into four Plan areas of development: Bridge Vista (Portway to 2nd Street), Urban Core (2nd to 16th Street), Civic

Greenway (16th to 41st Street), and Neighborhood Greenway (41st Street to east end of Alderbrook Lagoon).

During the Plan development, four community-wide forums, three open houses, and numerous community meetings were held at various locations within the four Plan areas. In addition, staff and/or consultants conducted stakeholder interviews, distributed and tabulated surveys. Development of the Vision Plan was structured to gain as much public input as possible. On December 7, 2009, after holding a final public hearing, the City Council accepted the Riverfront Vision Plan. For Fiscal Years 2011-2012, 2012-2013, 2013-2014, and 2014-2015, the City Council set goals to "Implement Riverfront Vision Plan on a Zone by Zone Basis."

With the completion of Phase 1 (Civic Greenway Area) and Phase 2 (Bridge Vista Area), staff has begun work on Phase 3 (Neighborhood Greenway Area). This Area encompasses the Alderbrook neighborhood north of Lief Erikson Drive to the pier headline between 41st Street and approximately 54th Street.

The City was notified that funding from the Department of Land Conservation and Development (DLCD) Coastal Management Technical Assistance Grants for the FY 2014-2015 grant cycle was awarded to the City to assist in code writing for implementation of Phase 3 of the Astoria Riverfront Vision Plan for the Neighborhood Greenway Area (41st Street to 54th Street). The grant funds need to be expended and the project complete by December 31, 2015.

The consultant team identified to work on this project is Angelo Planning Group. One of the project team members is Matt Hastie, who was directly involved in development of the Riverfront Vision Plan. The project includes public involvement opportunities held during Planning Commission work sessions. The final product would be code amendments and land use zoning map amendments which would ultimately be presented to the City Council for consideration of adoption.

Staff and the project team drafted preliminary code amendment language to address selected code issues for the Neighborhood Greenway Plan Area. The project team conducted a Town Hall meeting for interested community members on August 20, 2015. Approximately 70 people attended and provided staff with the initial public comments and questions. The Planning Commission held two public work sessions (September 1, 2015, and September 16, 2015) on the draft amendments with mailed, e-mailed, and published notification to the general public and to anyone who has expressed interest in the Riverfront Vision Plan implementation process. Updates were provided to the City Council on the progress made to date on July 27, 2015 and August 31, 2015. The work sessions have been well attended and public comments have been provided.

The RVP for the Neighborhood Greenway Plan Area identified Land Use Assumptions and Objectives which state that *"It is expected that very little overwater development will occur in the Neighborhood Greenway area in the future other than small-scale improvements such as docks or piers (or improvements to the River Trail trestle) which would enhance the general public access to the river."* The objectives of the RVP for this area include:

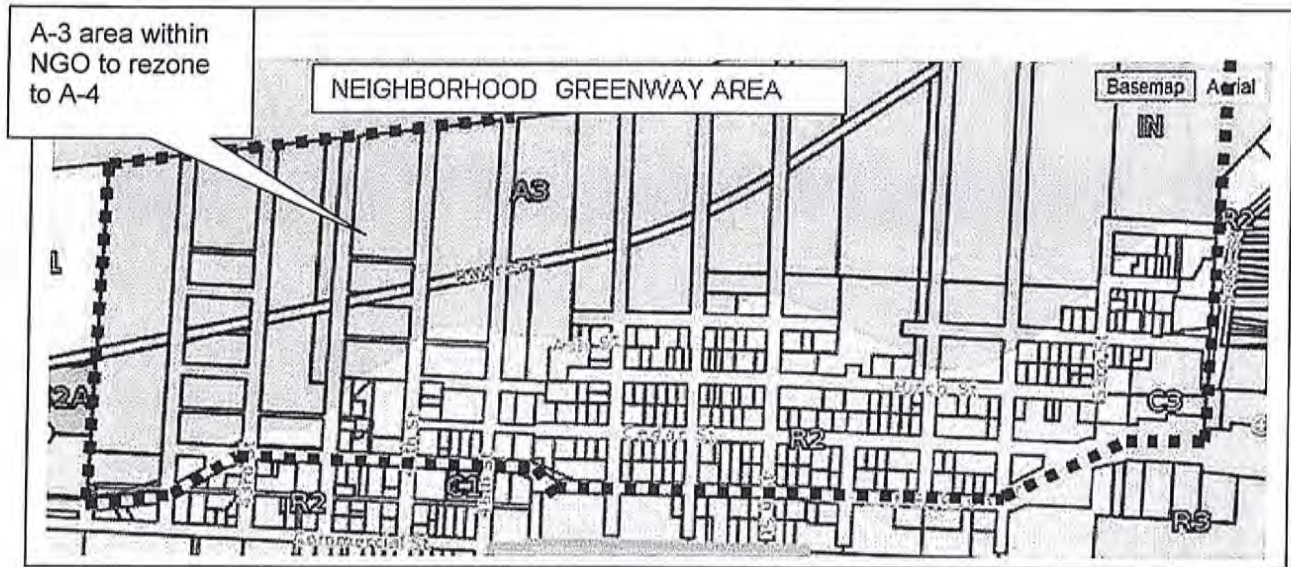
- Highlight the river's natural edge by framing views and creating path/trail systems that reflect the river's movement.
- Emphasize natural features and beauty along the edge of the river and lagoon.
- Use plantings and landscaping that provide riverbank restoration and increase habitat.
- Respect and protect the visual character of the Alderbrook Neighborhood.
- Minimize impacts of pedestrians on neighborhood residents.
- Protect river vistas to maintain physical and visual connection to the Columbia River.
- Create and maintain a sense of openness to enhance connections to the Columbia River and the Neighborhood Greenway area and improve the quality of residential areas. Open spaces should range from smaller, more intimate spaces to larger gathering places.
- Encourage maritime related uses consistent with Astoria's working riverfront such as docks, piers, marinas, and associated uses that provide jobs and maintain a historic connection to the river.

Throughout the RVP implementation process, the Planning Commission (APC) focused on these Assumptions and Objectives and did not attempt to change the Vision Plan as adopted.

At the work sessions, the Planning Commission ultimately provided guidance and recommendations on a set of proposed amendments to implement the Neighborhood Greenway Plan Area. Implementation of recommendations from the Riverfront Vision Plan in the Neighborhood Greenway Plan Area will take the form of Zoning Map amendments, Development Code amendments, and Comprehensive Plan amendments.

Proposed map amendments will include:

1. Rezone the aquatic parcels between 41st and approximately 54th Streets, from the shoreline to the pier head line from A-3 (Aquatic Conservation) to A-4 (Aquatic Natural).
2. Apply the new Neighborhood Greenway Overlay (BVO) Zone to the Neighborhood Greenway Plan Area.



Proposed Development Code text amendments will include:

1. Add a new Neighborhood Greenway Overlay Zone to address the standards for over-water development including structure height and width, allowable uses, and landscaping, etc.
2. Add new design standards for multi-family development in the Neighborhood Greenway Plan Area.
3. Add new design guidelines for multi-family residential and non-residential development in the Neighborhood Greenway Plan Area.
4. Establish landscaping standards for multi-family residential and non-residential construction/uses. There would be no landscaping standards for single-family and two-family dwellings.
5. Allow some exemptions for the few existing over-water buildings to continue to be viable businesses thereby preserving the historic structures.
6. Limit new, over-water development to maximum height of top of bank.
7. Make miscellaneous "housekeeping" amendments related to references to the above noted amendments.

Proposed Comprehensive Plan text amendments will include:

1. Update the description of the Alderbrook Area and reference the Neighborhood Greenway Overlay Area and Riverfront Vision Plan implementation.
2. Acknowledge the growing impact of traffic to the neighborhood.

3. Add a policy to investigate the possibility of extending the trolley to the Alderbrook area.
4. Change designation of aquatic area from conservation to natural, and amend allowable uses in the Aquatic Natural designated areas to include the exception for existing structures.

III. PUBLIC REVIEW AND COMMENT

A. Astoria Planning Commission

A public notice was mailed to Neighborhood Associations, various agencies, and interested parties on October 2, 2015. In accordance with ORS 227.186(5), a notice was mailed on October 2, 2015 to all property owners within the area and within 250' of the area proposed for the code and map amendments advising that ". . . the City of Astoria has proposed a land use regulation that may affect the permissible uses. . ." of their or other property. In accordance with Section 9.020, a notice of public hearing was published in the Daily Astorian on October 20, 2015. The proposed amendment is legislative as it applies City-wide.

B. State Agencies

Although concurrence or approval by State agencies is not required for adoption of the proposed amendments, the City has provided a copy of the draft amendments to representatives of the Oregon Departments of Transportation (ODOT), Land Conservation and Development (DLCD) and Department of State Lands (DLS) as part of the planning process.

IV. FINDINGS OF FACT

- A. Development Code Section 10.020.A states that *"an amendment to the text of the Development Code or the Comprehensive Plan may be initiated by the City Council, Planning Commission, the Community Development Director, a person owning property in the City, or a City resident."*

Development Code Section 10.020.B states that *"An amendment to a zone boundary may only be initiated by the City Council, Planning Commission, the Community Development Director, or the owner or owners of the property for which the change is proposed."*

Finding: The proposed amendments to the Development Code, Comprehensive Plan, and Astoria Land Use and Zoning Map are being initiated by the Community Development Director. The City Council has identified implementation of the Riverfront Vision Plan in their yearly goals since 2011. In addition, the City Council established a FY 2015-16 goal to "Continue implementation of the Riverfront Vision Plan."

B. Section 10.050.A states that *"The following amendment actions are considered legislative under this Code:*

1. *An amendment to the text of the Development Code or Comprehensive Plan.*
2. *A zone change action that the Community Development Director has designated as legislative after finding the matter at issue involves such a substantial area and number of property owners or such broad public policy changes that processing the request as a quasi-judicial action would be inappropriate."*

Finding: The proposed amendment is to amend the text of the Astoria Development Code Article 2 concerning Use Zones, and Article 14 concerning Overlay Zones. The amendment would create new overlay zone standards. The request is also to amend the Astoria Land Use and Zoning Map to rezone the aquatic area from A-3 (Aquatic Conservation) to A-4 (Aquatic Natural). The proposed amendment is also to amend the text of the Comprehensive Plan relative to the Riverfront Vision Plan.

The proposed amendments are applicable to a large area of the City and represent a relatively broad policy change. Processing as a legislative action is appropriate.

C. Section 10.070.A.1 concerning Text Amendments, requires that *"The amendment is consistent with the Comprehensive Plan."*

1. CP.005.5, General Plan Philosophy and Policy Statement states that local comprehensive plans *"Shall be regularly reviewed, and, if necessary, revised to keep them consistent with the changing needs and desires of the public they are designed to serve."*

Finding: The City accepted the Riverfront Vision Plan in 2009 as a long-range planning framework to address the changing needs and desires of the citizens concerning Riverfront development and the need to protect the environment. The City Council directed staff to initiate Development Code amendments to implement the Plan recommendations.

2. CP.010.2, Natural Features states that *"The City will cooperate to foster a high quality of development through the use of flexible development standards, cluster or open space subdivisions, the sale or use of public lands, and other techniques. Site design which conforms with the natural topography and protects natural vegetation will be encouraged. Protection of scenic views and vistas will be encouraged."*

Finding: The proposed amendments will implement the Riverfront Vision Plan for the Neighborhood Greenway Area. The amendments include design standards for residential development, design guidelines for non-

residential development, protection of scenic views and vistas, and protection of the natural aquatic areas.

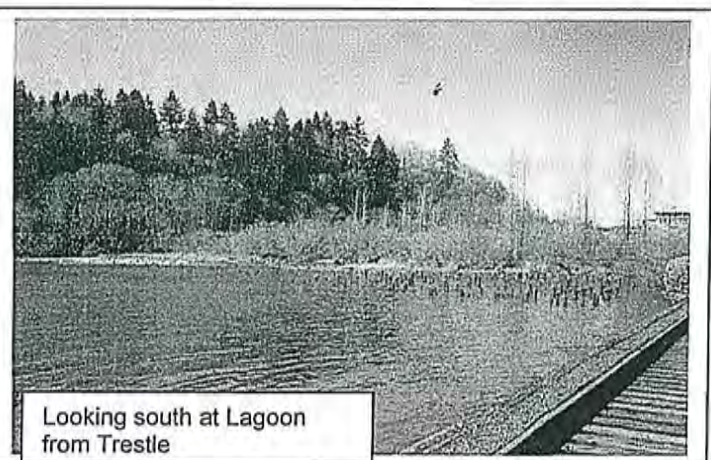
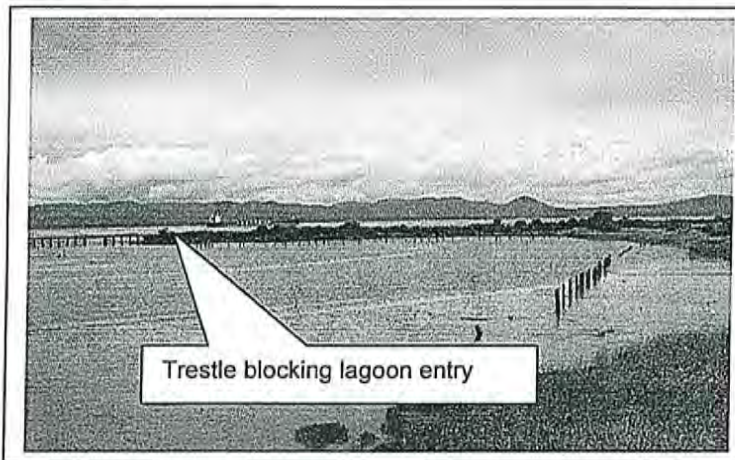
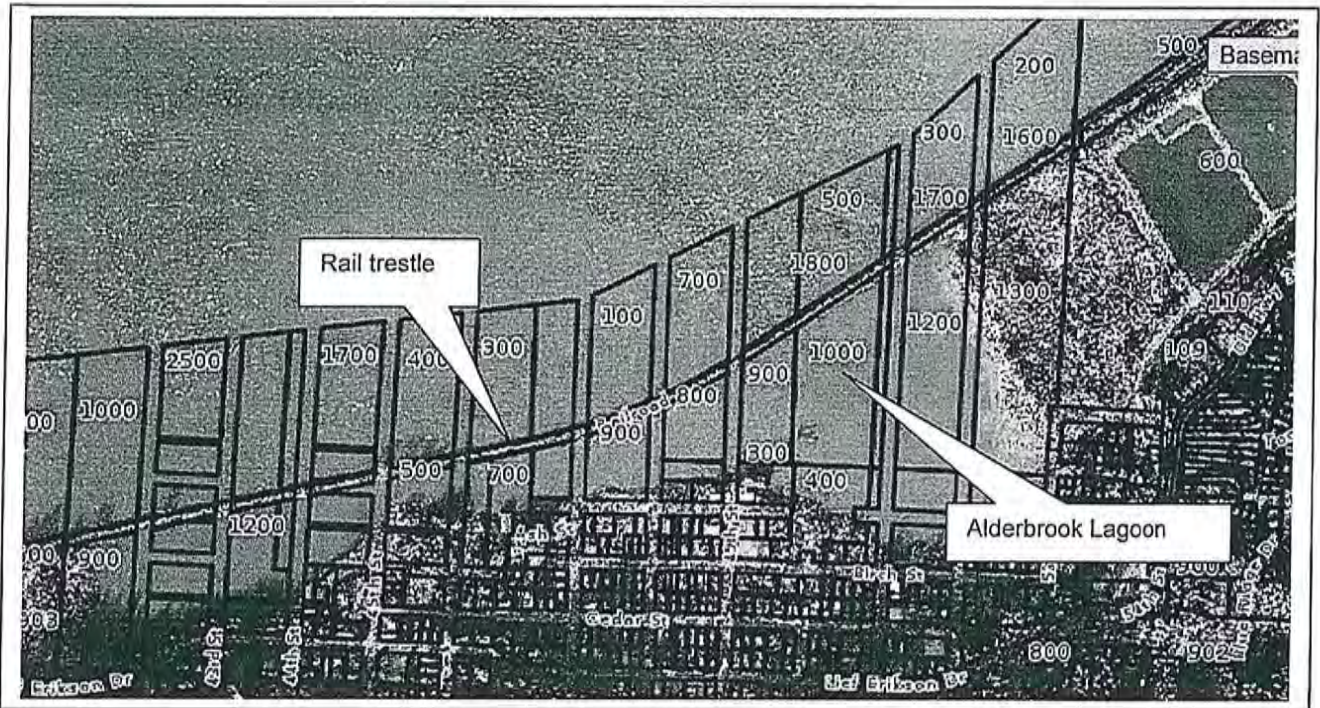
3. CP.015.1, General Land & Water Goals states that *"It is the primary goal of the Comprehensive Plan to maintain Astoria's existing character by encouraging a compact urban form, by strengthening the downtown core and waterfront areas, and by protecting the residential and historic character of the City's neighborhoods. It is the intent of the Plan to promote Astoria as the commercial, industrial, tourist, and cultural center of the area."*

CP.015.1, General Land & Water Goals states that *"Because of the City's strong water orientation, the Plan supports continuing regional efforts to manage the Columbia River estuary and shorelands. The City's land use controls, within this regional context, will be aimed at protecting the estuary environment and at promoting the best use of the City's shorelands."*

Finding: The proposed amendments create development standards for over-water construction and establish design standards and guidelines to protect the character of the Alderbrook residential neighborhood. This addresses the need to encourage a compact urban form. The design and landscaping standards protect the historic character of the City and waterfront areas. The reduction in allowable uses and development on parcels extending over the water, and the use of native vegetation will help protect the estuary environment. The proposed ordinance is intended to provide the guidance to help achieve these goals.

4. CP.203, Economic Development Goal 4 and Goal 4 Policies, goal states *"Continue to encourage water-dependent industries to locate where there is deep water, adequate back-up space, and adequate public facilities."* Policies states *"1. Maintain areas of the City in order to provide sufficient land for water dependent as well as non-water dependent industries."*

Finding: The aquatic area in the Neighborhood Greenway Area is known as Alderbrook Lagoon. The shallow lagoon is blocked from access by large vessels due to the railroad trestles. The lagoon could accommodate small vessels such as canoes and kayaks. The area does not have sufficient upland area that is zoned to support water-dependent industries as there is no shoreland zone and the land area is zoned R-2 (Medium Density Residential). In addition, the lagoon is currently zoned A-3 (Aquatic Conservation which does not support development.



The proposed rezone to A-4 (Aquatic Natural) would eliminate a few allowable uses such as marinas, aquaculture facilities, low-intensity water-dependent commercial or industrial uses, and in-water log storage. However, as noted above, the area is not conducive to such development due to physical and access constraints and upland zoning. Public piers and docks would still be allowed to accommodate the smaller vessels.

The requirements for shoreland and estuary development in Development Codes Articles 4 and 5 would remain applicable to any development in this area.

5. CP.210.1, Economic Element, Economic Development Recommendations, states that *"In the City's waterfront areas, the City will continue to promote a combination of tourist oriented development, industrial development associated with the City's working waterfront and water-related and dependent industries, and distribution and sales of goods and services for*

Astoria residents and businesses. These efforts will be guided by and consistent with the Astoria Riverfront Vision Plan.”

Finding: As noted above, this area of the waterfront is not conducive to development and is not accessible to the Columbia River for large vessels. The Riverfront Vision Plan for this area recognizes the value of the natural setting and identifies the following related goals:

- *“Emphasize natural features and beauty along the edge of the river and lagoon.*
- *Respect and protect the visual character of the Alderbrook Neighborhood.*
- *Protect river vistas to maintain physical and visual connection to the Columbia River.*
- *Create and maintain a sense of openness to enhance connections to the Columbia River and the Neighborhood Greenway area and improve the quality of residential areas. Open spaces should range from smaller, more intimate spaces to larger gathering places.*
- *Encourage maritime related uses consistent with Astoria’s working riverfront such as docks, piers, marinas, and associated uses that provide jobs and maintain a historic connection to the river.”*

The Vision Plan emphasizes the natural features and openness and while it notes encouragement of maritime related uses, the lagoon cannot support more intense uses such as marinas due to its shallow depth, lack of upland support areas, and limited access from the Columbia River. The proposed rezone to A-4 (Aquatic Natural) would preserve the natural features while still allowing the lower impact uses such as docks and piers for small vessels.

6. CP.025.2, Policies Pertaining to Land Use Categories and Density Requirements, states that *“Changes in the land use and zoning map may be made by boundary amendment so long as such change is consistent with the goals and policies of the Comprehensive Plan.*

Factors to be considered when evaluating requests for zoning amendments will include compatibility with existing land use patterns, effect on traffic circulation, adequacy of sewer, water and other public facilities, contiguity to similar zones, proposed buffering, physical capability including geologic hazards, and general effect on the environment.”

Finding: Consistency with the goals and policies of the Comprehensive Plan are addressed in this Section of the Findings of Fact. The factors are addressed in this Section and Sections D & E below of the Findings of Fact.

7. CP.085.2, Alderbrook Area Policies, states that *"The residential character of Alderbrook will be protected through the designation of the aquatic area from 41st Street to Tongue Point as conservation, and by the present zoning pattern. Development in the 100-year flood area shall be subject to the requirements of the City's Flood Hazard Overlay Zone."*

CP.175.B, Uppertown / Alderbrook Subarea Plan, Aquatic Features, states that *"The aquatic portions of this subarea include open water and nearshore habitats in the river and Alderbrook Cove. The aquatic characteristics of the open water areas are similar to the adjacent channel (see Estuary Channels Subarea Plan). In the nearshore habitats, sediments become finer and benthic infauna productivity higher. A 1980 study reported high densities of amphipods in nearshore areas of the subarea. Juvenile fall Chinook salmon migrate in the shallow nearshore habitats. Alderbrook Cove contains fine sediments and probably has high benthic productivity. The Cove is partially fringed by low elevation tidal marshes. Bird use on the marshes and adjacent tidal flats is high."*

CP.175.F, Uppertown / Alderbrook Subarea Plan, Aquatic and Shoreland Designations, states that *"The aquatic area between 29th and 41st Streets is designated Development to the pierhead line, except at the East End Mooring Basin where the designation corresponds to the outer boundary of the pier. East of 41st Street, the aquatic area is designated Conservation..."*

Finding: The current designation as "Conservation" does allow for some minor development. The goal of the Riverfront Vision Plan is to protect the natural features of this area. The area is proposed to be rezoned to "Natural" which is similar but restricts some of the more intrusive human uses. Sections CP.085.2 and CP.175.F of the Comprehensive Plan is proposed to be amended to state that the area from 41st to approximately 54th Street be designated as "Natural".

8. CP.175.G.1, Uppertown / Alderbrook Subarea Plan, Subarea Policies, states that *"The Alderbrook area has unique characteristics and values. Plan amendments which would allow higher-intensity uses than those now present are discouraged."*

CP.150.B, Permitted Uses in Columbia River Estuary Aquatic and Shoreland Designations, Natural Aquatic, states that *"Natural Aquatic areas are designated to assure the protection of significant fish and wildlife habitats; of continued biological productivity within the estuary; and of scientific, research, and educational needs. These areas are managed to preserve natural resources in recognition of dynamic, natural, geological, and evolutionary processes. Natural Aquatic areas include all major tidal marshes, tide flats, and seagrass and algae beds. The designation is intended to preserve those aquatic natural resource systems existing relatively free of human influence."*

CP.150.C.29, Permitted Uses in Columbia River Estuary Aquatic and Shoreland Designations, Conservation Aquatic, lists the following as an allowable use *"In pile supported buildings existing as of October 1, 2002, non-water dependent and non-water related uses."*

Finding: Currently, there are a few over-water buildings that have existed for over 100 years. In 2002, the City amended (A02-02) the Development Code for the A-3 Zone and the Comprehensive Plan to allow these buildings to be used for other than maritime related uses. At that time, there were three buildings, all within the Alderbrook lagoon, and which were separated from the main Columbia River channel by the railroad tracks. If the Lagoon is rezoned to A-4, it is proposed that the same allowable use notation would be included in the A-4 Zone and in Section CP.150.D for the Natural Aquatic designation. The only buildings that would be affected by this amendment would be the same three buildings as in 2002.

9. CP.068, Astoria Riverfront Vision Overlay Area Policies, states that

- "1. *Promote physical and visual access to the river. The overall Comprehensive Plan objectives are to:*
- a. *Maintain current areas of open space and create new open space areas.*
 - b. *Provide for public access to the river within private developments.*
 - c. *Retain public ownership of key sites along the riverfront.*
 - d. *Protect view sheds along the river, including corridors and panoramas from key viewpoints.*
 - e. *Use alternative development forms (e.g., clustered development, narrower, taller profiles, setbacks, stepbacks, and gaps in building frontages) to preserve views."*

Finding: The proposed amendments implement the Riverfront Vision Plan. They identify areas for limited development to top of bank height maximum to minimize their impact on public access and the preservation of the natural features.

- "2. *Encourage a mix of uses that supports Astoria's "working waterfront" and the City's economy. The overall Comprehensive Plan objectives are to:*
- a. *Maintain the authentic feel of the riverfront.*
 - b. *Prioritize siting of water-related businesses along the river.*
 - c. *Allow for some residential development along the riverfront, emphasizing smaller-scale work force (moderate income) housing.*
 - d. *Allow for development that supports downtown and other commercial areas.*
 - e. *Limit development in areas with most significant impacts on open space, view or other resources.*

- f. *Promote uses that provide jobs and support the local economy.”*

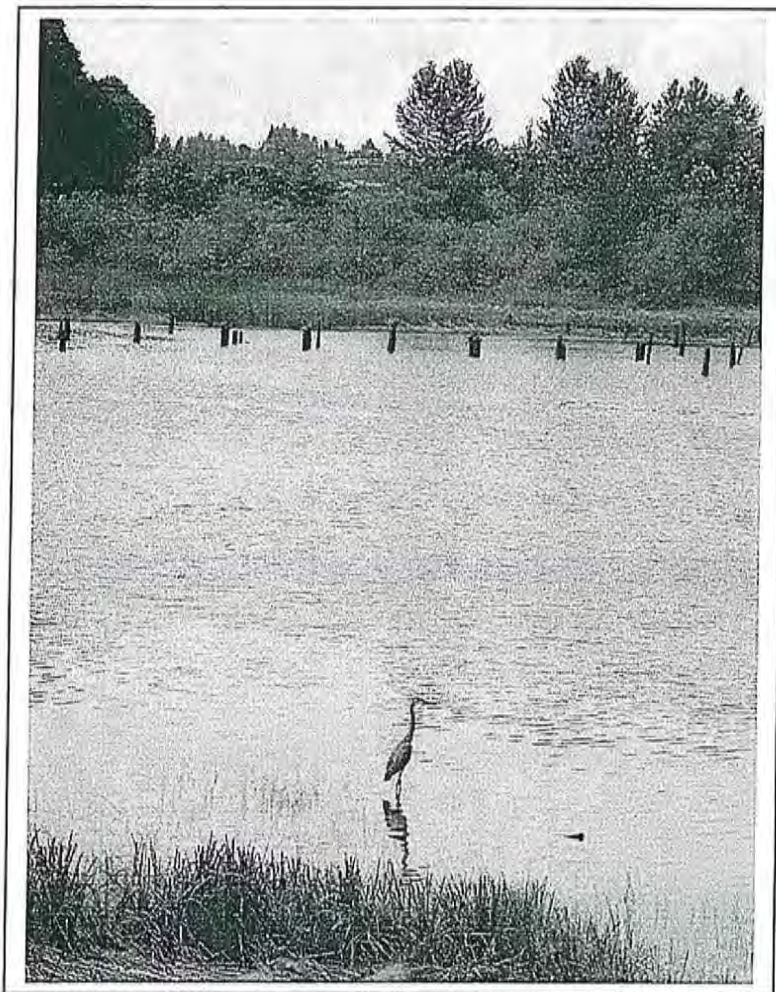
Finding: The proposed amendments include rezoning of the aquatic area from A-3 to A-4 to further preserve the natural features. Opportunities for a variety of uses that support the City's working waterfront have been integrated in Comprehensive Plan policies and Development Code provisions for the Civic Greenway and Bridge Vista areas, with similar provisions possibly proposed to be applied in the Urban Core area in a future code amendment process for that area.

- “3. *Support new development that respects Astoria's historic character. The overall Comprehensive Plan objectives are to:*
 - a. *Enhance or refine Development Code to achieve vision principles.*
 - b. *Implement design review, design standards, or other tools to guide the appearance of new development.*
 - c. *Devote resources to rehabilitating old structures.”*

Finding: The proposed amendments would create clear and objective design review standards for multi-family development and design review guidelines for multi-family residential and non-residential development that reflect the character of the Alderbrook. The proposal would allow for repair, restoration, and reconstruction of existing over-water buildings.

- “4. *Protect the health of the river and adjacent natural areas. The overall Comprehensive Plan objectives are to:*
 - a. *Protect natural areas for wildlife viewing.*
 - b. *Replace invasive plants with native species.*
 - c. *Incorporate natural elements in the design of future public and private improvements.”*

Finding: The proposed amendments would encourage the use of native plants along the Riverfront using the list of recommended plants listed in Development Code Article 3. Landscaping requirements would encourage and/or require placement of plant material that would enhance the riverfront and viewing opportunities. The Alderbrook Lagoon area is proposed to have a maximum height limit of top of bank and be rezoned from A-3 (Aquatic Conservation) to A-4 (Aquatic Natural) to further protect the natural features.



- “5. *Enhance the River Trail. The overall Comprehensive Plan objectives are to:*
- a. *Maintain, repair, extend, and enhance the River Trail.*
 - b. *Provide better pedestrian connections between the downtown and the riverfront.*
 - c. *Create amenities such as shelters, lighting, and public restrooms in targeted locations.*
 - d. *Ensure adequate parking opportunities along, adjacent to, and near the riverfront.*
 - e. *Address safety issues associated with mix of autos, pedestrians, trolley, and other activities.*
 - f. *Ensure long-term maintenance of public improvements.”*

Finding: River Trail construction in Alderbrook is completed. The proposed amendments would establish design and siting standards and guidelines to protect the River Trail from incompatible encroachments. It would require public access to the riverfront, require recorded maintenance agreements for certain landscaping features, and allow for the continuation of the River Trail. The amendments would also allow some flexibility in landscaping by allowing a percentage to be devoted to public amenities such as benches, restrooms, interpretive signage, etc.

10. CP.185.H, Regional Estuary and Shoreland Policies, Fisheries and Aquaculture Policies, states that *"Policies in this subsection apply to all projects that could conceivably affect fisheries (either commercial or recreational) or aquaculture in the Columbia River Estuary. This subsection is also applicable to the development of aquaculture facilities and to fisheries enhancement projects. . .*
2. *Sufficient space for present and anticipated needs shall be reserved for the following uses:*
- Fishing vessel moorage;
Seafood receiving and processing;
Boat repair;
Gear storage;
Ice making;
Cold storage;
Other seafood industry support facilities. . .*
7. *A number of sites in Astoria and throughout the Columbia River Estuary are suitable for development or expansion of facilities for the commercial seafood industry. These include moorage, fuel, ice, fish receiving and processing, gear storage, marine hardware sales and repair, boat building and repair, and related facilities. The City will periodically consider amending its Comprehensive Plan as needed to provide sufficient sites for these facilities. The City will rely on information in periodic updates of the Columbia River Estuary Regional Management Plan pertaining to fisheries-related development in the estuary.*

Finding: The Alderbrook Lagoon area is currently zoned A-3 and proposed to be rezoned to A-4. Development in either zone is limited. The following uses currently allowed in the A-3 zone would be eliminated with the A-4 Zone: water-dependent parts of an aquaculture facility; high-intensity water-dependent recreation including boat ramp, marina, and individual dock; mining and mineral extraction; low-intensity water-dependent commercial or industrial use requiring occupation of water-surface area by means other than fill; and in-water log storage. As noted earlier, the Lagoon is shallow, not accessible from the Columbia River by large vessels, and does not have sufficient upland area to support development. Other areas of the Riverfront are better suited to this type of development and the City currently allows for those types of development in those areas. The Lagoon is more appropriate as a natural area as identified in the RVP.

11. CP.185.O, Residential, Commercial and Industrial Development Policies, states that *"Policies in this subsection are applicable to construction or expansion of residential, commercial or industrial facilities in Columbia River Estuary shoreland and aquatic areas. Within the context of this subsection, residential uses include single and multifamily structures,*

mobile homes, and floating residences (subject to an exception to Oregon Statewide Planning Goal 16). Duck shacks, recreational vehicles, hotels, motels and bed-and-breakfast facilities are not considered residential structures for purposes of this subsection. Commercial structures and uses include all retail or wholesale storage, service or sales facilities and uses, whether water-dependent, water-related, or non-dependent, non-related. Industrial uses and activities include facilities for fabrication, assembly, and processing, whether water-dependent, water-related or non-dependent, non-related.

1. *New non-water-dependent uses in aquatic areas and in Marine Industrial Shorelands shall not preclude or pose any significant conflicts with existing, proposed or probable future water-dependent uses on the site or in the vicinity.*
2. *Residential, commercial or industrial development requiring new dredging or filling of aquatic areas may be permitted only if all of the following criteria are met:*
 - a. *The proposed use is required for navigation or other water-dependent use requiring an estuarine location, or if specifically allowed in the applicable aquatic designation; and*
 - b. *A substantial public benefit is demonstrated; and*
 - c. *The proposed use does not unreasonably interfere with public trust rights; and*
 - d. *Feasible alternative upland locations do not exist; and*
 - e. *Potential adverse impacts are minimized.”*

Finding: The proposed amendments would limit the allowable uses in the aquatic area to uses that would support the Natural designation. Uses would be limited to maximum top of bank height which would accommodate piers, docks, and low-impact recreation in the Lagoon. Commercial development would be restricted to the land area within the R-2 Zone. These proposed uses are consistent with this Comprehensive Plan section which protects the waterfront area for the low-impact marine uses. Any project proposed would be subject to compliance with this section at the time of project proposal.

12. CP.186.C, Cumulative Impacts, Cumulative Impact Analysis, states that

1. *Public Access.*

Activities generating cumulative impacts on public access can both enhance and reduce opportunities for public access to the waters and shorelines of the Columbia River Estuary. Public access is treated broadly here to include both physical and visual access. . .

Boat ramps and marinas have a strongly beneficial cumulative impact on public access for the boating public. Private individual moorages on the other hand can have negative cumulative impacts

with respect to public access if allowed to overcrowd particular waterways. Continuous development of individual moorages along a reach of the Columbia River Estuary or a tributary can block public shoreline access and inhibit small boat navigation, having a strongly negative cumulative impact. The regional estuarine construction policies and standards encourage community docks and piers and discourage individual moorages. . .

5. *Recreation/Tourism.*

Discussion of cumulative impacts on recreation and tourism includes estuary-oriented recreation undertaken by both local residents and by visitors from outside the region. Many impacts may be largely aesthetic in nature. . .

Boat ramps, marinas, and moorages have a generally positive impact on recreation and tourism, though there may also be a negative aesthetic component. The net cumulative impact is probably positive, however, because the estuary is large relative to the extent of existing recreational boat facilities. . .

Finding: The proposed amendments would limit the Alderbrook Lagoon to low-impact water-related and water-dependent uses such as small boat docks and ramps. It would also limit the height to top of bank to protect the important public view and natural character of the area, consistent with RVP objectives. This would support boat ramps, etc. that are considered to be a positive impact on recreation and tourism. The proposed amendments are intended to minimize the cumulative negative impacts along the Riverfront by preserving some vistas and views.

The cumulative impacts of the River Trail and Alderbrook Lagoon Park development to the neighborhood should be considered. Alderbrook has several dead end streets especially around the Park area. A Comprehensive Plan amendment is proposed that recognizes this issue.

13. CP.185.M, Regional Estuary and Shoreland Policies, Public Access Policies, states that *"Public access" is used broadly here to include direct physical access to estuary aquatic areas (boat ramps, for example), aesthetic access (viewing opportunities, for example), and other facilities that provide some degree of public access to Columbia River Estuary shorelands and aquatic areas."*

CP.185.M.2 to M.5, Regional Estuary and Shoreland Policies, Public Access Policies, states that

- "2. *Public access in urban areas shall be preserved and enhanced through waterfront restoration and public facilities construction, and other actions consistent with Astoria's public access plan.*

3. *Proposed major shoreline developments shall not, individually or cumulatively, exclude the public from shoreline access to areas traditionally used for fishing, hunting or other shoreline activities.*
4. *Special consideration shall be given toward making the estuary accessible for the physically handicapped or disabled.*
5. *Astoria will develop and implement programs for increasing public access."*

CP.185.N.2, Regional Estuary and Shoreland Policies, Recreation and Tourism Policies, states that *"Recreation uses in waterfront areas shall take maximum advantage of their proximity to the water by: providing water access points or waterfront viewing areas; and building designs that are visually u {typo from original ordinance} with the waterfront."*

CP.204, Economic Development Goal 5 and Goal 5 Policies, Goal states *"Encourage the preservation of Astoria's historic buildings, neighborhoods and sites and unique waterfront location in order to attract visitors and new industry."* The Policy 1 states *"Provide public access to the waterfront wherever feasible and protect existing access. The importance of the downtown waterfront in terms of aesthetics, public access and business improvement cannot be overemphasized. The City supports the concept of the "People Places Plan," and encourages local organizations in the construction and maintenance of waterfront parks and viewing areas."*

Finding: One of the reasons the Riverfront Vision Plan was developed was to enhance public access to the estuary and allow for preservation of public open space and park areas along the Columbia River. Public access includes both physical and visual access. The River Trail along the Columbia River is used by locals as well as visitors and is maintained for its aesthetic values as well as for its transportation values. The Neighborhood Greenway Area was identified as an area to emphasize the natural features and allow minimal over-water development while preserving visual and public access. The Urban Core Area was identified for more intense development, Bridge Vista for mixed development and open space, and the Civic Greenway Area was identified for more open space. The proposed rezone to A-4 would protect the natural features and promote public access.

The proposed implementation of the RVP will allow for limited over-water development of minimal maritime related facilities while protecting public visual and physical access to the River. The maximum height of structures is proposed to be at existing shoreline bank height which would limit the type of development that could occur.

The existing over-water structures are proposed to be allowed to continue and include uses that would provide access to the River by the public. The proposed amendment would limit those uses to the existing structures only.

14. CP.185.G, Estuary and Shoreland Policies states that *"This subsection applies to uses and activities with potential adverse impacts on fish or wildlife habitat, both in Columbia River estuarine aquatic areas and in estuarine shorelands.*
1. *Endangered or threatened species habitat shall be protected from incompatible development.*
 2. *Measures shall be taken protecting nesting, roosting, feeding and resting areas used by either resident or migratory bird populations.*
 3. *Major nontidal marshes, significant wildlife habitat, coastal headlands, and exceptional aesthetic resources within the Estuary Shorelands Boundary shall be protected. New uses in these areas shall be consistent with the protection of natural values, and may include propagation and selective harvest of forest products, grazing, harvesting, wild crops, and low intensity water-dependent recreation."*

CP.460.1, Natural Resource Policies states that *"The Plan land and water use designations will protect those areas that have high natural value, and direct intensive development into those areas that can best support it."*

CP.460.3, Natural Resource Policies states that *"The City recognizes the importance of "trade offs" that must occur in the planning process. Although certain estuary areas have been designated for intensive development, other areas will be left in their natural condition in order to balance environmental and economic concerns."*

Finding: The Alderbrook Lagoon is a significant wildlife habitat area. The proposed amendment to rezone the Lagoon to A-4 allows for some over water development while excluding high-intensity commercial uses that could be located elsewhere on land areas. This zone change would further protect the natural features and wildlife area. It encourages and/or requires the use of native plants along the Riverfront. The standards maintain open areas for protection of the estuary habitat and to maintain vistas and views.

15. CP.204.3 & CP.204.4, Economic Development Goal 5 and Goal 5 Policies, Goal states *"Encourage the preservation of Astoria's historic buildings, neighborhoods and sites and unique waterfront location in order to attract visitors and new industry."* The Policies state
3. *Encourage the growth of tourism as a part of the economy.*
 - a. *Consider zoning standards that improve the attractiveness of the City, including designation of historic districts, stronger landscaping requirements for new construction, and Design Review requirements.*
 4. *Protect historic resources such as downtown buildings to maintain local character and attract visitors."*

CP.250.1, Historic Preservation Goals states that *"The City will Promote and encourage, by voluntary means whenever possible, the preservation,*

restoration and adaptive use of sites, areas, buildings, structures, appurtenances, places and elements that are indicative of Astoria's historical heritage."

CP.250.3, Historic Preservation Goals states that *"The City will Encourage the application of historical considerations in the beautification of Astoria's Columbia River waterfront.*

CP.200.6, Economic Development Goals states that the City will *"Encourage the preservation of Astoria's historic buildings, neighborhoods and sites and unique waterfront location in order to attract visitors and new industry."*

CP.205.5, Economic Development Policies states that *"The City encourages the growth of tourism as a part of the economy. Zoning standards which improve the attractiveness of the city shall be considered including designation of historic districts, stronger landscaping requirements for new construction, and Design Review requirements."*

Finding: The proposed amendments will adopt design standards to allow for development that is consistent with the design of the residential Alderbrook Area. This neighborhood has many older buildings similar to other parts of Astoria, but has not been inventoried for historic designation. However the unique characteristic of this neighborhood is proposed to be protected by the addition of design standards for multi-family dwellings and design guidelines for non-residential development that is compatible with the existing development within the area. No standards are proposed for the single-family and/or two-family residential development.

The River and River Trail are important tourism/economic assets for the City and will be protected from incompatible development with the proposed amendments. The proposed amendments exempt the existing historic over water buildings from some of the requirements so as to encourage and support the restoration and adaptive reuse of these buildings. However, additions to these buildings would be subject to the proposed development standards. The code would also protect the scenic views of the Columbia River waterfront with a top of bank height limitation for over-water development.

16. CP.270, Parks, Recreation, and Open Space Element, Goals states that *"The City of Astoria will work:*
1. *To develop a balanced park system.*
 2. *To reflect Astoria's special qualities and characteristics. . .*
 5. *To provide or encourage waterfront parks. . .*
 7. *To promote general beautification. . .*
 12. *The City will continue its efforts to improve public access to the shoreline through:*
 - a. *The construction of public access points, pathways, and street ends;*

- b. *The encouragement of public access projects in conjunction with private waterfront development actions, possibly through the use of local improvement districts and/or grant funds; and*
- c. *The protection of street ends and other public lands from vacation or sale where there is the potential for public access to the water. The City will work with the Division of State Lands (DSL) to determine the status of submerged and submersible lands adjacent to the City street ends."*

Finding: The City has established a River Trail along the Columbia River as a City park. The Riverfront Vision Plan identifies this as a public area and encourages protection of a portion of the public views and vistas in the Neighborhood Greenway Area. The RVP for the Neighborhood Greenway Planning Area identified Land Use Assumptions and Objectives which state *"Highlight the river's natural edge by framing views and creating path/trail systems that reflect the river's movement. Emphasize natural features and beauty along the edge of the river and lagoon. . . Protect river vistas to maintain physical and visual connection to the Columbia River. Create and maintain a sense of openness to enhance connections to the Columbia River and the Neighborhood Greenway area and improve the quality of residential areas. Open spaces should range from smaller, more intimate spaces to larger gathering places. . ."*

The proposed amendments address the design, location, height, etc. for development on both the water and land. The proposed amendments also address public amenities and the ability of a developer to provide specific public amenities in conjunction with their development and promote the general beautification of the waterfront area. The limitation of structure height and reduction in allowable uses in the Lagoon would protect the waterfront park from incompatible intrusions. There are three City-owned parks in the area. These properties would be protected as public access areas.

- 17. CP.470.1, Citizen Involvement states that *"Citizens, including residents and property owners, shall have the opportunity to be involved in all phases of the planning efforts of the City, including collection of data and the development of policies."*

Finding: Throughout the process of drafting the proposed ordinance, the City has provided extensive public outreach. The APC has held two work sessions and one town hall meeting over the last two months with invitations and notices sent to interested parties, neighborhood associations, stakeholders, email lists, web site, etc. Anyone interested in the proposed ordinance was encouraged to submit suggestions and comments. Work sessions were open for discussion with the public to allow for interactive feedback at this early stage of the adoption process. The following is a list of public work sessions, public hearings, and newspaper articles concerning the draft ordinance. There were numerous "Letters to the Editor" in the Daily Astorian which are not listed.

August 19, 2015	Daily Astorian article "Town Hall Meeting set for Neighborhood Greenway
August 20, 2015	Town Hall Meeting
August 21, 2015	Daily Astorian article "Neighborhood Greenway gets closer look"
August 24, 2015	Daily Astorian editorial "Alderbrook discussion puts community wishes first"
September 1, 2015	APC work session
September 16, 2015	APC work session
October 27, 2015	APC public hearing

The City was very conscious of the interest in protection of the Riverfront and the need to have an ordinance that would meet the needs of the citizens, property owners, protect the environment and historic resources, be in compliance with State regulations, and would be a permit process that was easy for both the citizens and staff.

Finding: The request is consistent with the Comprehensive Plan.

- D. Section 10.070.A.2 concerning Text Amendments requires that *"The amendment will not adversely affect the ability of the City to satisfy land and water use needs."*

Section 10.070.B.2 concerning Map Amendments requires that *"The amendment will: a. Satisfy land and water use needs; or . . ."*

Finding: The proposed amendment will satisfy land use needs in that it will allow for the development of private properties while protecting the vistas and views along the Neighborhood Greenway Area of the River Trail. The proposed amendment limits the allowable over-water development in this area thereby reducing some of the impacts associated with a more intensive development. The water area is zoned A-3 (Aquatic Conservation) with limited allowable development, all of which are maritime related. The area is proposed to be rezoned to A-4 (Aquatic Natural) which would eliminate a few of the more intensive uses and would better reflect the current condition of the enclosed shallow lagoon. No changes are proposed to the allowable uses on land.

An amendment to Comprehensive Plan Section CP.080, Alderbrook Area, is proposed to state *"However, the limited access into the Alderbrook Area from Lief Erikson Drive creates concerns with increased traffic to the area with no secondary vehicular outlets."* which would acknowledge the existing transportation issues with the increase in traffic and a single access neighborhood. In addition, CP.085.8, Alderbrook Area Policies, is proposed to be amended by the addition to read as follows: *"8. Extension of the trolley service to the Alderbrook neighborhood should be investigated and considered."* The possible addition of trolley service to the area would potentially provide an alternative mode of transportation thereby reducing the impacts on the transportation system.

Proposed lighting and open space landscaping standards would decrease impacts to Police and Fire protection services by the creation of appropriately lit and open areas. The Aquatic area was not included in the Buildable Lands Inventory as buildable area, and there are no proposed changes to the residential area, therefore, the proposed amendment will not adversely affect the ability of the City to satisfy land and water use needs.

- E. Section 10.070.B.2 concerning Map Amendments requires that *"The amendment will . . .*
- b. *Meet transportation demands. The amendment shall be reviewed to determine whether it significantly affects a transportation facility pursuant to Section -0060 of Oregon Administrative Rule (OAR) 660-012, the Transportation Planning Rule (TPR). When the City, in consultation with the applicable roadway authority, finds that a proposed amendment would have a significant effect on a transportation facility, the City shall work with the roadway authority and applicant to modify the request or mitigate the impacts in accordance with the TPR and applicable law; or*
 - c. *Provide community facilities and services."*

Finding: The Neighborhood Greenway area is located on the north side of Lief Erikson Drive between 41st and approximately 54th. It is currently partially developed with mostly residences, but also includes a few commercial facilities such as Crest Motel, a construction company, art studio, Alderbrook Station over-water structure, and a few home occupations. There are three City park facilities. There are only a few undeveloped lots in the existing R-2 Zone as noted in the Buildable Lands Inventory, several of which are owned by the City. The aquatic area proposed for a map amendment fronts the River Trail and the Neighborhood Greenway Area of the Riverfront Vision Plan.

There are no traffic lights in this area. Alderbrook is generally accessed from Lief Erikson Drive (Hwy 30) at 45th Street. In accordance with Statewide Planning Goal 12 concerning Transportation, and the Transportation Planning Rule (TPR) (OAR 660-12-060), any plan amendment having a significant effect on a transportation facility (i.e. Highway 30) must assure that the allowed land uses are consistent with the function, capacity, and level of service of the facility. In addition, OAR 734-051-0080, and OAR 734-051-0100 state that a proposed development or land use action where an on-site review indicates that operational or safety concerns may be present requires a Traffic Impact Study.

The proposed rezone of the aquatic area would reduce a few of the more intense allowable uses and therefore would reduce potential transportation impacts associated with future development. Since no changes are proposed for the R-2 Zone, there are no changes to the transportation impacts associated with future development.

All City utility services are available to the area. There is no indication that operational or safety concerns are present, nor would they be increased as a result of the proposed uses on the existing transportation system. Any future

development would be subject to a Traffic Impact Study as required by Development Code Article 3.

In April 2014, the City Council adopted the Transportation System Plan (TSP). This Plan was conducted by the City of Astoria in conjunction with the Oregon Department of Transportation (ODOT) and studied the existing and forecasted transportation needs in the City. The subject aquatic area proposed for rezone is located on north of the shoreline in the Alderbrook area which is north of Lief Erikson Drive between 41st and 54th Street. There were several projects identified in the TSP for the Neighborhood Greenway Area as follows:

D-4 – US 30 Speed Warning System – Install a speed warning system that activates when a motorists approaches at a high speed

D-7 – US 30 / 45th Street Safety Enhancement – Install eastbound and westbound left-turn pockets on US 30

D-8 – US 30 / 54th Street Safety Enhancement – Provide an eastbound left turn pocket on US 30

D-37 – 54th Street-Old US Hwy 30 Upgrade – Improve to a Working collector street cross-section

CR-02 – US 30 and 45th Street – Upgrade existing crossing to the highest level pedestrian actuated beacon approved by ODOT

P-14 – Lief Erikson Drive (East) Sidewalk Infill – Complete sidewalk gaps on north side of the street from 46th to 50th and both sides from 50th to 54th

B-10 – 45th Street Shared Roadway Enhancements – Add wayfinding and shared lane markings

B-11 – 51st Street Shared Roadway Enhancements – Add wayfinding and shared lane markings

B-16 – Birch Street Shared Roadway Enhancements – Add wayfinding and shared lane markings

B-18 – Cedar Street Shared Roadway Enhancements - Add wayfinding and shared lane markings

These projects are aimed at pedestrian and bike safety and to create better vehicle and pedestrian circulation in the Neighborhood Greenway Area. The proposal to rezone the aquatic area to A-4 would not impact transportation and would be consistent with these projects

The area proposed to be zoned A-3 to A-4 is accessed from City streets and not directly from the State Highway. Therefore, ODOT would not typically comment

on the TPR review. ODOT has been included in the draft amendment review process. Based on the existing TSP and projected traffic volumes and projected uses, the transportation facilities in this area, including any proposed future improvements, will be sufficient to accommodate the uses allowed in the proposed A-4 Zone areas.

F. Oregon Administrative Rules Section 660-012-0060 (Plan and Land Use Regulation Amendments) states that:

1. *“(1) If an amendment to a functional plan, an acknowledged comprehensive plan, or a land use regulation (including a zoning map) would significantly affect an existing or planned transportation facility, then the local government must put in place measures as provided in section (2) of this rule, unless the amendment is allowed under section (3), (9) or (10) of this rule. A plan or land use regulation amendment significantly affects a transportation facility if it would:
 - (a) *Change the functional classification of an existing or planned transportation facility (exclusive of correction of map errors in an adopted plan);”**

Finding: As shown in the attached draft Ordinance, Attachment B, the proposed Land Use and Zoning Map amendment consists of the following:

- Rezoning an aquatic area of A-3 (Aquatic Conservation) to A-4 (Aquatic Natural)

This area is adjacent to roadways including Ash Street, Alder Street, Birch Street, and the street ends of 45th through 53rd Streets. These rights-of-way are designated as “Residential Local Streets” as shown in Figure 15 (Multi-Modal Street System) of the 2013 City of Astoria Transportation System Plan (TSP). The proposed Land Use and Zoning Map amendment does not propose or necessitate changes to these classifications.

2. *“(b) Change standards implementing a functional classification system; or . . .”*

Finding: Section 6 of the Astoria TSP establishes design guidelines, spacing standards, and mobility targets for City streets based on functional classification. The proposed Land Use and Zoning Map amendment does not propose or necessitate changes to these guidelines, standards, or targets.

3. *“(c) Result in any of the effects listed in paragraphs (A) through (C) of this subsection based on projected conditions measured at the end of the planning period identified in the adopted TSP. As part of evaluating*

projected conditions, the amount of traffic projected to be generated within the area of the amendment may be reduced if the amendment includes an enforceable, ongoing requirement that would demonstrably limit traffic generation, including, but not limited to, transportation demand management. This reduction may diminish or completely eliminate the significant effect of the amendment.

- (A) *Types or levels of travel or access that are inconsistent with the functional classification of an existing or planned transportation facility;*
- (B) *Degrade the performance of an existing or planned transportation facility such that it would not meet the performance standards identified in the TSP or comprehensive plan; or . . .”*

Finding: The proposed Land Use and Zoning Map amendment only affects the aquatic area known as Alderbrook Lagoon. It would reduce the allowable uses limiting some of the more intense commercial uses currently allowed. In general, as proposed, the amendment would allow similar uses or uses that are not expected to generate any more trips than existing zoning. The number of trips generated would be expected to decrease. Therefore, the proposed map amendment should not significantly change the character of land use and zoning in the area and, thus, the changes in zoning would not affect the types and levels of travel and performance of transportation facilities. More detailed information about specific changes in allowed uses is provided in Table E.1 which presents examples of uses currently permitted in the A-3 and A-4 Zones, as well as some uses that are proposed to be permitted or prohibited. The only change proposed for the A-4 Zone is to allow the existing over-water buildings as noted in the A-3 Zone to be allowed to continue. Since no new uses are proposed and a few of the more intense uses are eliminated, no additional analysis of the transportation impacts is required.

Table E.1 A-3 AQUATIC CONSERVATION and A-4 AQUATIC NATURAL ZONE Allowable Use Comparison

Use	A-3		A-4	
	Outright	Conditional Use	Outright	Conditional Use
Estuarine enhancement.	X			
Riprap for protection of use existing as of October 7, 1977, unique natural resources, historical or archeological resources, or public facility.	X			
Maintenance and repair of existing structure or facility.	X			X
Active restoration of fish habitat,	X			X

Use	A-3		A-4	
	Outright	Conditional Use	Outright	Conditional Use
wildlife habitat, or water quality.				
Filling in conjunction with any of the permitted uses 1 through 4, above, pursuant to the applicable standards in Section 4.070.	X			X for repair of existing structure
Tidegate installation and maintenance in existing functional dike.	X		X	
Dredging to obtain fill material for dike maintenance pursuant to the dike maintenance dredging standards.	X			
Pipeline, cable, and utility crossing.	X			X
Water-dependent parts of an aquaculture facility which do not involve dredge or fill or other estuarine alterations other than incidental dredging for harvest of benthic species or removable in-water structures such as stakes or racks.	X			
Dredging in conjunction with any of the permitted uses 1 through 9, above, pursuant to the applicable standards in Section 4.050.	X			X for specific conditional uses
Navigation aid.	X		X	
Communication facility.	X			X
Bridge crossing support structure.	X			X
Boat ramp for public use where no dredge or fill is needed for navigational access.	X			X
Undeveloped low intensity water-dependent recreation.	X		X	
Project for the protection of habitat, nutrient, fish, wildlife and aesthetic resources.	X			
Research and educational observation.	X		X	
Piling and pile supported structure in conjunction with any of the permitted uses 1 through 17 above.	X			X for specific conditional uses
Passive restoration.	X		X	
Bridge crossing.	X		X	
Transportation facilities, excluding electric car charging stations.	X			X

Use	A-3		A-4	
	Outright	Conditional Use	Outright	Conditional Use
Aquaculture and water-dependent portions of aquaculture facility.		X		X limited to temporary removable structure
Active restoration for purposes other than protection of habitat, nutrient, fish, wildlife and aesthetic resources.		X		X
Temporary alteration.		X		X
Beach nourishment at sites designated in the Comprehensive Plan.		X		
Filling in conjunction with conditional uses 1 through 4, above, pursuant to the applicable standards in Section 4.070.		X		X for specific conditional uses
High-intensity water-dependent recreation including boat ramp, marina, and individual dock.		X		
Minor navigational improvement.		X		
Mining and mineral extraction.		X		
Dredging in conjunction with any of the conditional uses 1 through 8, above, pursuant to the applicable standards in Section 4.050.		X		X for specific conditional uses
Low-intensity water-dependent commercial or industrial use requiring occupation of water-surface area by means other than fill.		X		
In-water log storage.		X		
Piling in conjunction with any of the conditional uses 1 through 11, above.		X		
Temporary use meeting the requirements of Section 3.240.		X		
In pile supported buildings existing prior to October 1, 2002, non-water-dependent or non-water-related uses as follows:		X		Proposed
a. Arts and crafts studios.		X		Proposed
b. Bed and breakfast, home stay lodging, or inn.		X		Proposed
c. Home occupation.		X		Proposed

Use	A-3		A-4	
	Outright	Conditional Use	Outright	Conditional Use
d. Professional and business office, personal service establishment limited to beauty and barber services and garment alterations.		X		Proposed
e. Residential home.		X		Proposed
f. Single-family dwelling.		X		Proposed
g. Two-family dwelling.		X		Proposed
h. Multi-family dwelling.		X		Proposed
i. Off-street parking requirements for the above uses may be located in the upland zone adjacent to the use. The Planning Commission may impose additional landscape buffering to protect the adjacent residential uses.		X		Proposed

4. *“(C) Degrade the performance of an existing or planned transportation facility that is otherwise projected to not meet the performance standards identified in the TSP or comprehensive plan.”*

Finding: As concluded in the Future Needs Analysis (Section H, Volume 2) in the Astoria TSP, *“In addition, the . . . US 30 / Nimitz Drive-Maritime Road intersections are expected to operate with a level of service “F” for the side street, even though they are expected to meet ODOT’s v/c target. . .”* This intersection is just east of the Neighborhood Greenway Area. It was also noted that in the area between the Astoria-Megler Bridge and Nimitz Drive-Maritime Road, *“Each of the segments along US 30 has more driveways and public street approaches than allowed to comply with the access spacing standards.”* Since no changes are proposed to the existing transportation system, and no new uses are proposed, criterion is not applicable.

Finding: The proposed amendments comply with the Oregon Administrative Rules Section 660-012-0060 (Plan and Land Use Regulation Amendments) requirements.

V. CONCLUSION AND RECOMMENDATION

The request is consistent with the Comprehensive Plan and Development Code. Staff recommends that the Planning Commission recommend adoption of the proposed amendments to the City Council.

Alternatively, the Planning Commission can continue the hearing until November to allow additional comment, make revisions to the proposed amendments, or close the hearing and deliberate further. However, as a result of a grant deadline, and extensive outreach completed to date with the Alderbrook neighborhood, staff recommends approval subject to additional public testimony.

ORDINANCE NO. 15-_____

AN ORDINANCE AMENDING THE ASTORIA DEVELOPMENT CODE AND LAND USE AND ZONING MAP PERTAINING TO IMPLEMENTATION OF THE ASTORIA RIVERFRONT VISION PLAN IN THE NEIGHBORHOOD GREENWAY AREA

THE CITY OF ASTORIA DOES ORDAIN AS FOLLOWS:

Section 1. Section 14.130 through 14.138 pertaining to the Neighborhood Greenway Overlay Zone is added to read as follows:

“NGO: NEIGHBORHOOD GREENWAY OVERLAY ZONE

14.130. PURPOSE.

The purpose of the Neighborhood Greenway Overlay Zone is to implement the land use principles of the Astoria Riverfront Vision Plan, dated December 2009, as they pertain to the Neighborhood Greenway Plan Area. The Neighborhood Greenway Overlay (NGO) Zone is intended to protect views of and access to the Columbia River, provide for an enhance open space and landscaping, support limited water-dependent uses consistent with Astoria's working waterfront. The NGO Zone extends from approximately 41st Street to the east side of Alderbrook neighborhood at approximately 54th Street and between Lief Erikson Drive and the pier head line of the Columbia River as depicted on the City's Zoning Map.

14.131. APPLICABILITY AND REVIEW PROCEDURES.

The provisions of the Neighborhood Greenway Overlay Zone shall apply to all new construction or major renovation, where “major renovation” is defined as construction valued at 25% or more of the assessed value of the existing structure, unless otherwise specified by the provisions in this Ordinance.

Review of applications in the Neighborhood Greenway Overlay Zone is subject to the administrative procedures and approval of the Community Development Director established in Article 9.

A. Residential Development Exception

These standards shall not apply to single-family and two-family dwellings.

B. Residential Development

Applications for multi-family dwellings may be reviewed administratively subject to the Design Review Standards in Section 14.134 or through the public design review process subject to the Design Review Guidelines in Section 14.135.

C. Non-Residential and Mixed Use Development.

Applications shall be reviewed through the public design review process subject to the Design Review Guidelines in Section 14.135.

14.132. ALLOWABLE USES FOR OVERWATER DEVELOPMENT.

Outright and Conditional uses within the Neighborhood Greenway Overlay Zone shall be limited to allowable uses in the A-4 Zone (Aquatic Natural) with the following exceptions:

1. In pile supported buildings existing prior to October 1, 2002, non-water-dependent or non-water-related uses as follows are allowed as a conditional use:
 - a. Arts and crafts studios.
 - b. Bed and breakfast, home stay lodging, or inn.
 - c. Home occupation.
 - d. Professional and business office, personal service establishment limited to beauty and barber services and garment alterations.
 - e. Residential home.
 - f. Single-family dwelling.
 - g. Two-family dwelling.
 - h. Multi-family dwelling.
 - i. Off-street parking requirements for the above uses may be located in the upland zone adjacent to the use. The Planning Commission may impose additional landscape buffering to protect the adjacent residential uses.

14.133. STANDARDS FOR OVERWATER DEVELOPMENT.

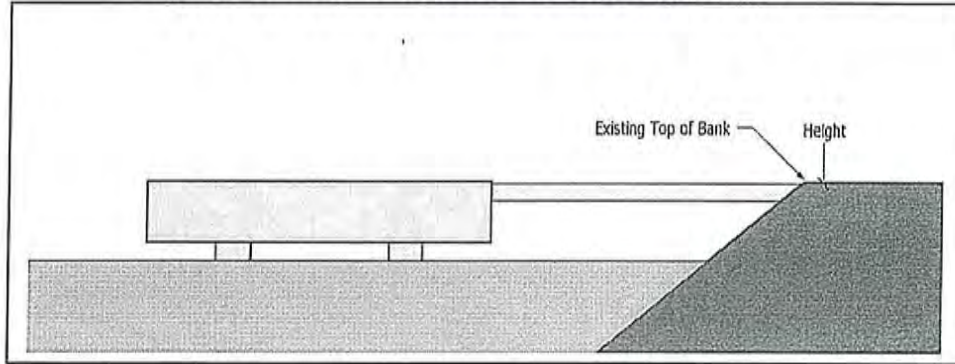
The following development standards apply to overwater development in the Neighborhood Greenway Overlay Zone. The Overwater Development standards shall also apply to on-land development north of the River Trail and/or 50' wide railroad line property between 41st Street and approximately 54th Street. In the event of a conflict between this Section and other Sections of the Astoria Development Code, this Section shall control.

Maintenance, repair, or restoration of buildings existing prior to 2002 shall be exempt from the standards of this Section. Additions and/or new construction on these buildings shall be subject to these standards.

A. Height.

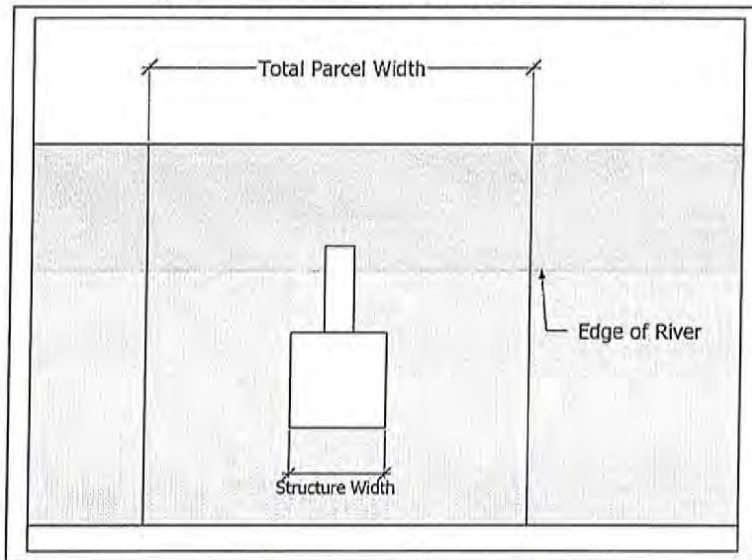
1. Maximum building height, except hand rails, shall be the top of the existing adjacent riverbank. No variance may be granted for an exception to this height limitation.

Figure 14.133-1: Maximum Building Height



- B. The maximum width of an overwater structure is 25% of the total parcel width (measured along the parcel frontage adjacent to the Columbia River) or 50 feet, whichever is smaller. In cases where total parcel width is 100 feet or less, the structure width may be up to 25 feet.

Figure 14.133-2: Maximum Building Width



14.134. RESIDENTIAL DEVELOPMENT DESIGN STANDARDS.

A. Applicability.

1. Residential.

- a. Single-family and two-family dwellings are not subject to the design standards.
- b. Multi-family dwellings proposed in the Neighborhood Greenway Overlay Zone may be reviewed in accordance with one of two review options:

- (1) Applications in compliance with Multi-Family Development Design Standards of Section 14.134 shall be reviewed pursuant to procedures for administrative review by the Community Development Director established in Article 9 unless the applicant requests review through the City's Design Review Committee pursuant to design review guidelines in Section 14.135; or
- (2) If a multi-family development applicant requests review through the City's Design Review Committee process, applications shall be reviewed in accordance with the Residential Development Design Guidelines of Section 14.135 and shall be reviewed pursuant to design review procedures in Article 9 and 14.

2. Non-Residential.

Non-residential uses may be reviewed pursuant to procedures for administrative review by the Community Development Director established in Article 9; or they may be reviewed pursuant to the Residential Development Design Guidelines of Section 14.135 and design review procedures in Article 9 and 14. The applicant shall have the option to choose between these two options.

3. Figures.

Figures included in Section 14.133 through 14.135 are included for illustrative purposes only and are not intended to be regulatory in and of themselves. If there is an inconsistency between the Figure and the Development Code text, the text shall prevail.

B. Design Standards.

1. Building Forms.

- a. All buildings shall be based on a rectangular or square form.
- b. All multi-family dwelling unit buildings shall have a front porch, at least six (6) feet deep and 60 square feet in area.

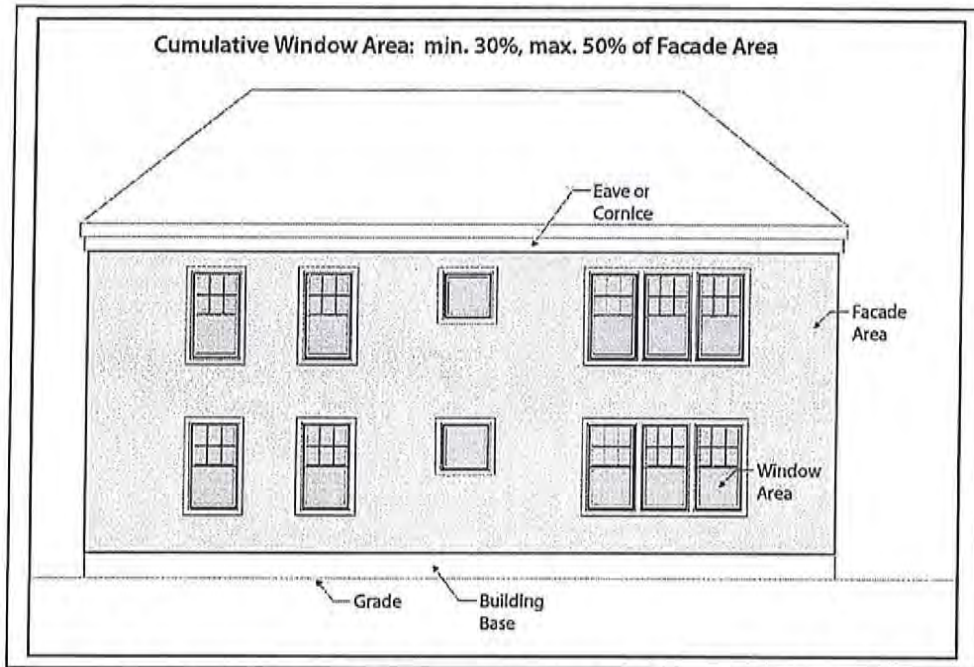
2. Window Design.

The following design standards apply to all facades.

- a. Windows required. All facades facing a right-of-way, River Trail, or common open space shall have windows.
- b. Window area. Window area shall cover a minimum of 30% of all street-

facing facade areas and shall not exceed 50% of street-facing facade areas.

Figure 14.134-1: Window Area



c. Window lites. Window lite design shall be one of the following:

- 1) Single-lite windows; or
- 2) Multiple-lite true-divided windows; or
- 3) Combination of single and multiple-lite true-divided windows; or
- 4) Applied muntins with profile facing window exterior.

Figure 14.134-2: Window Lites

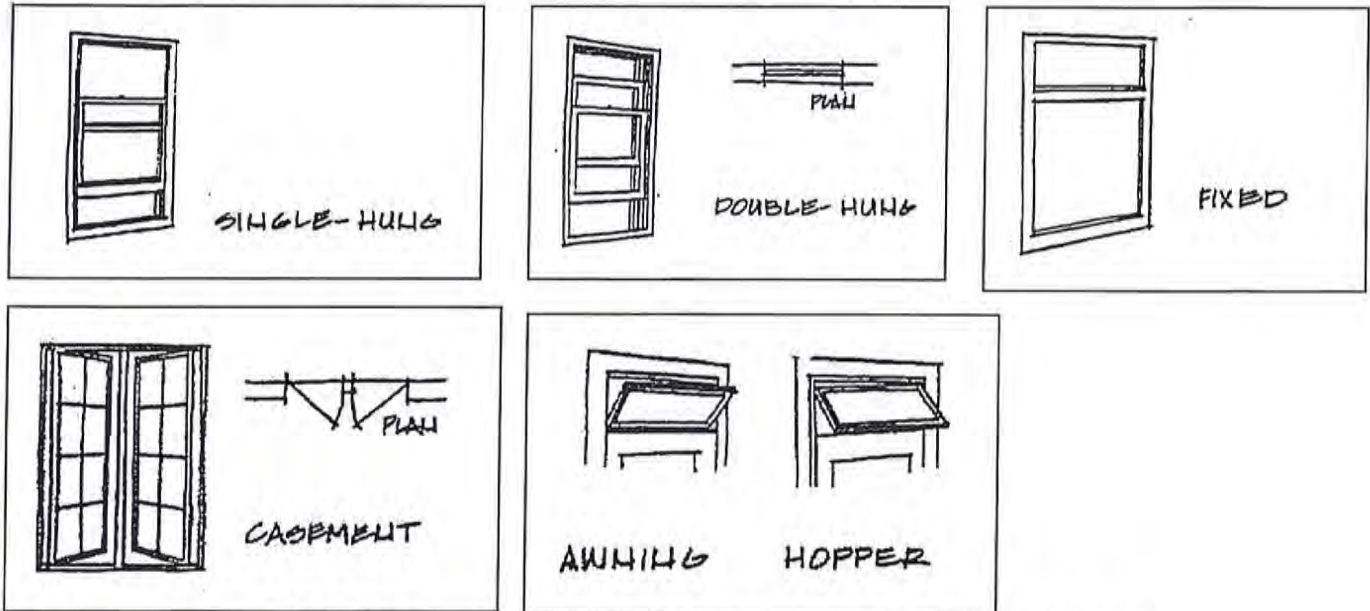


d. Windows shall be fixed or open in one of the following configurations:

- 1) Fixed window; or
- 2) Single-hung windows; or

- 3) Double-hung windows; or
- 4) Awning or hopper windows; or
- 5) Casement windows.

Figure 14.134-3: Fixed and Opening Windows

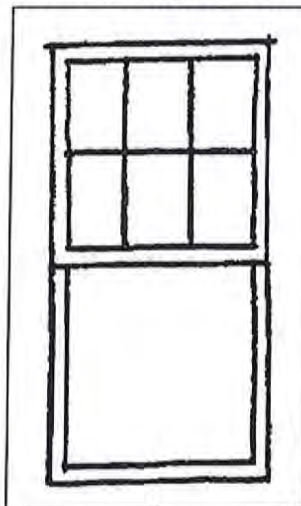


e. Window shape. Window shape shall be one of the following:

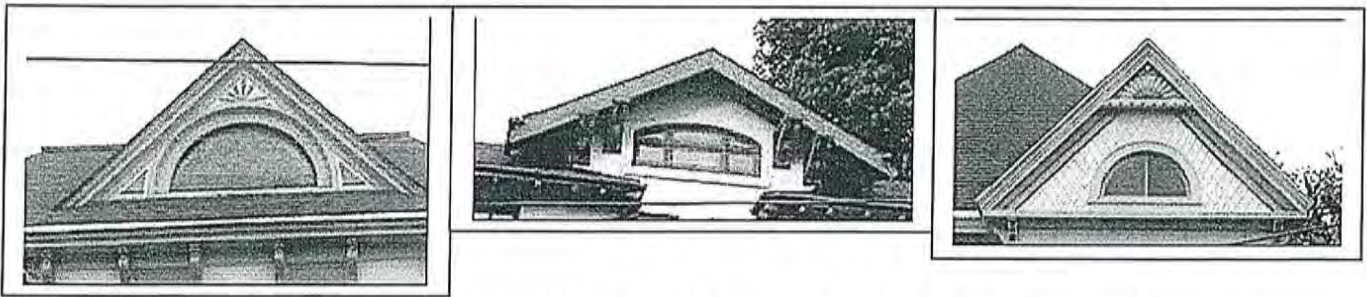
- 1) Vertical rectangle; or
- 2) Square.
- 3) Arched or decorative windows are permitted but should not exceed more than 30% of the total window coverage on all facades of the building.

Figure 14.134-4: Window Shapes

Vertical rectangular window

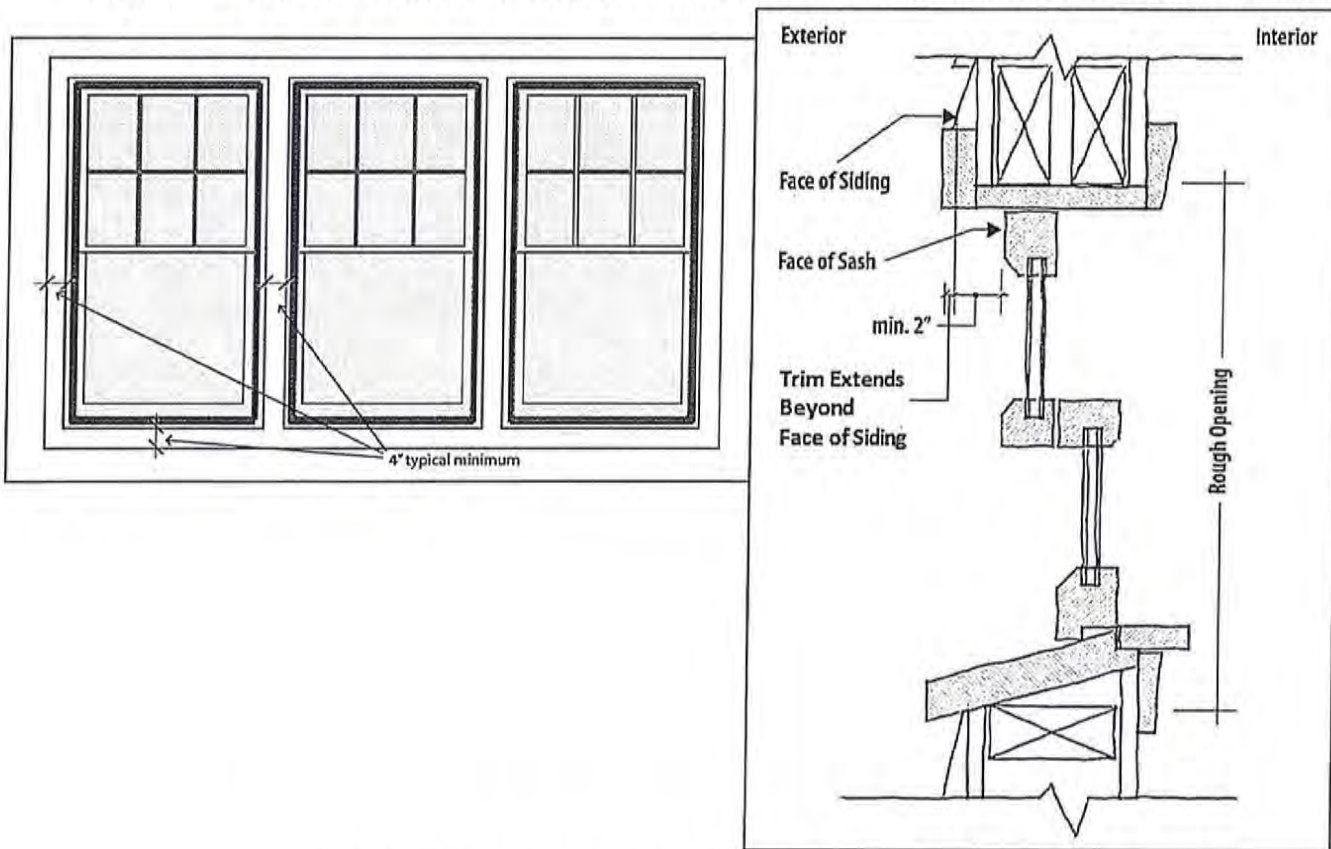


Examples of arched or decorative windows



- f. Window detailing. Windows shall have casings/trim, sills, and crown moldings. Window detailing shall meet the following requirements.
- 1) Casings/trim shall have minimum dimensions of 5/4 inch x 4 inch and shall extend beyond the facade siding.
 - 2) Windows shall be recessed a minimum distance of two (2) inches from the trim surface to ensure a shadow line/effect.

Figure 14.134-5: Window Detailing – Trim and casement location and dimensions

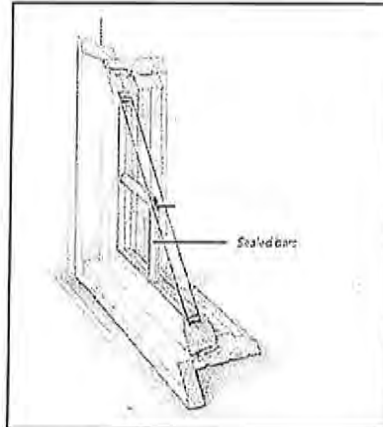


- g. Window design prohibited. The follow window design features are prohibited.

- 1) Applied muntins that have no profile.
- 2) Smoked, tinted, or frosted glass, except for bathroom windows not on the street-facing facade.
- 3) Mirrored glass.

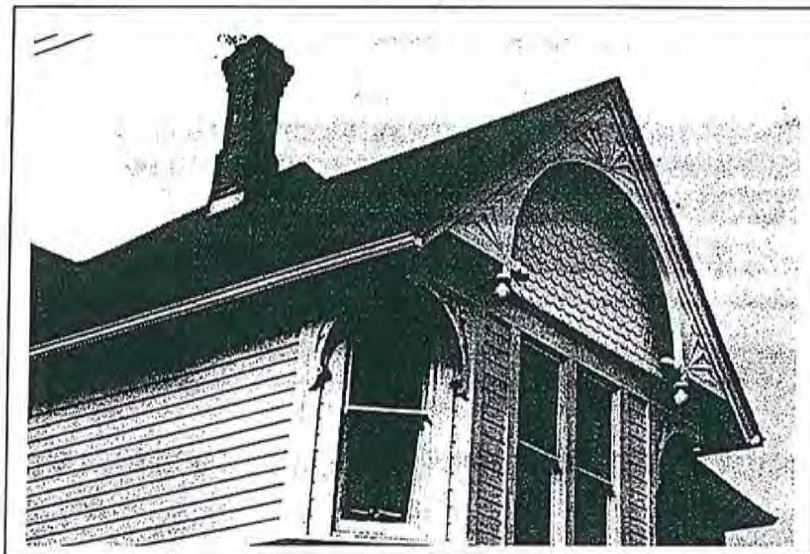
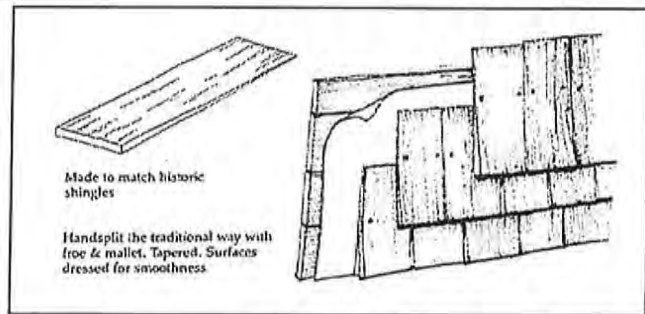
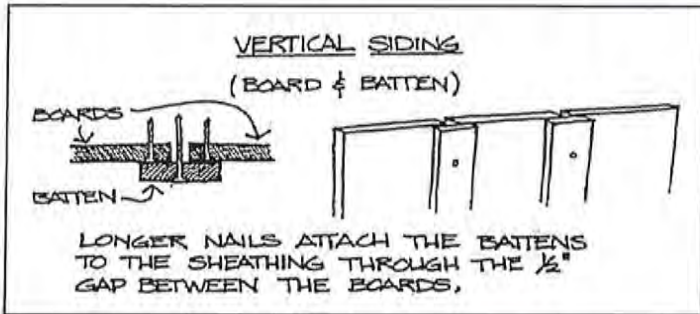
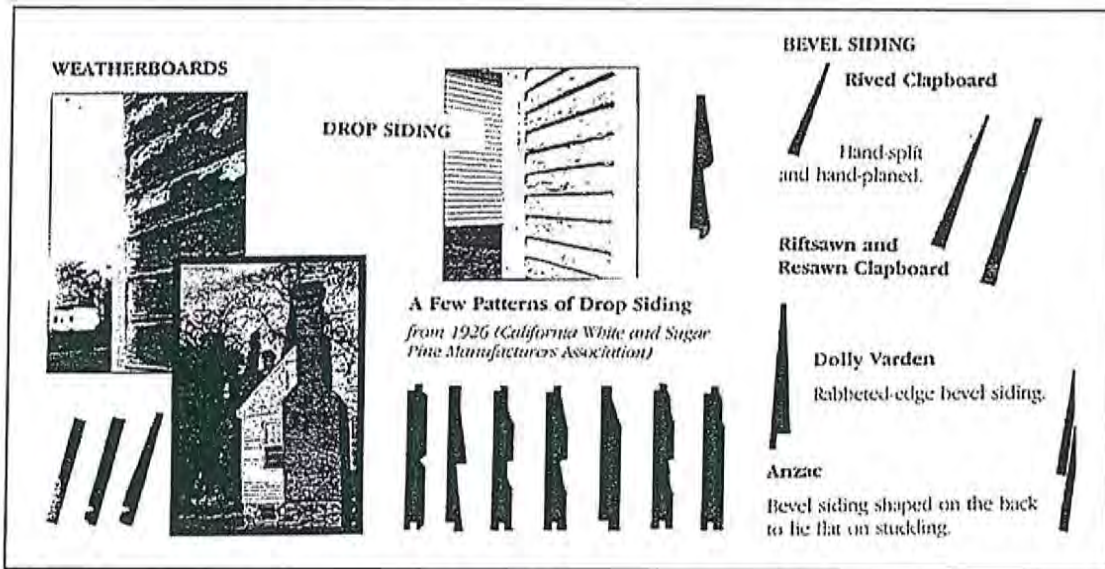
Figure 14.134-6: Window Design Prohibited

Muntins with no profile



3. Exterior Wall Treatments and Materials.
 - a. A minimum of 80% of exterior walls shall be constructed of one or more of the following sets of treatments and materials.
 - 1) Drop siding; or
 - 2) Weatherboard siding; or
 - 3) Clapboard; or
 - 4) Rectangular wood shingle or shake; or
 - 5) Decorative wood shingle or shake; or
 - 6) Board and batten.
 - b. Horizontal siding shall have six inches or less exposure.
 - c. Vertical board and batten shall have true battens.

Figure 14.134-7: Exterior Walls – Permitted Materials



- d. Paneled material shall be applied in a manner which avoids the occurrence of seams along the wall plane. Where seams cannot be avoided, they shall be located in a manner that relates logically to windows and other architectural features of the facade. Horizontal seams shall be covered by a trim board or cornice piece.

Figure 14.134-8: Exterior Walls – Seam Treatment

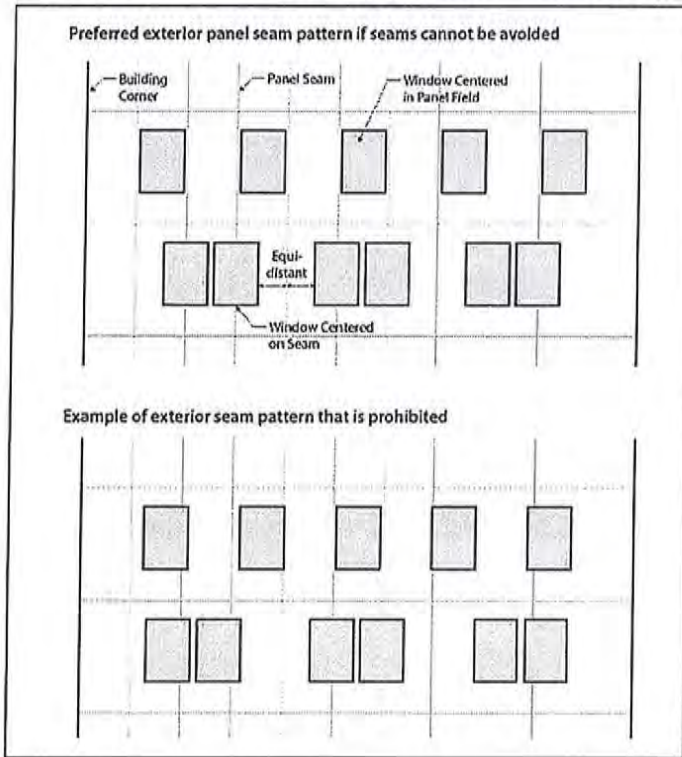
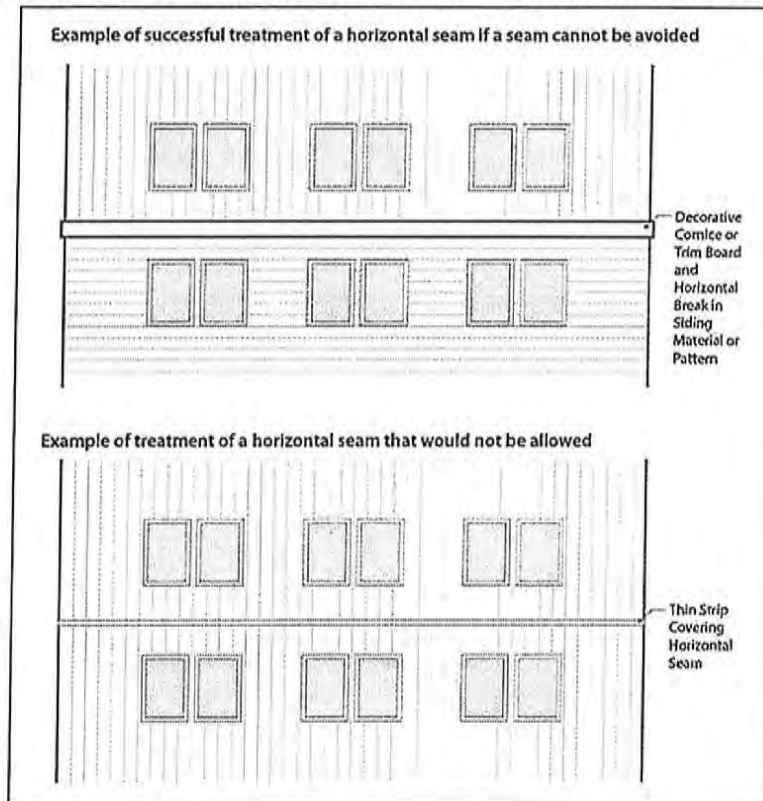


Figure 14.134-9: Exterior Walls – Horizontal Seam Treatment

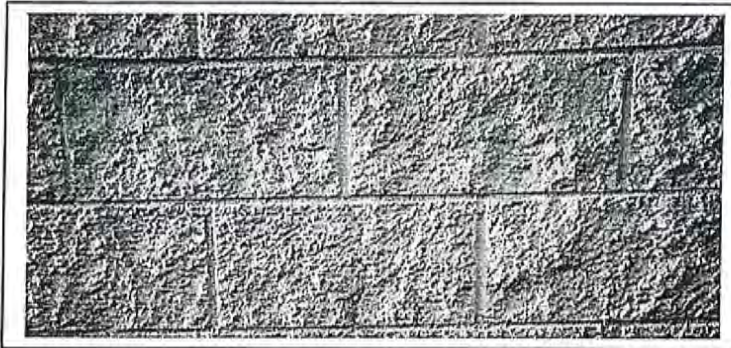


e. Exterior wall treatments and materials prohibited. The following types of treatments and materials are prohibited.

- 1) Exposed textured concrete block.
- 2) Flagstone or other applied stone products.
- 3) Precast concrete or decorative concrete panels.
- 4) Plywood paneling.

Figure 14.134-10: Exterior Wall Treatments and Materials Prohibited

Textured concrete



Applied stone



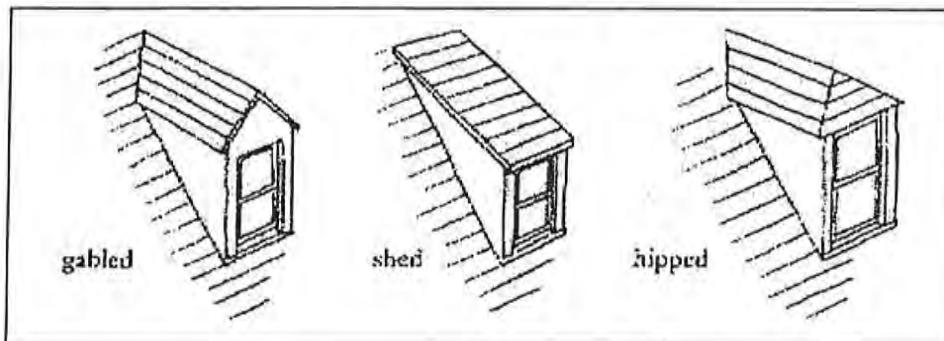
4. Roof Elements.

a. Roof elements permitted. The following roof design elements are permitted.

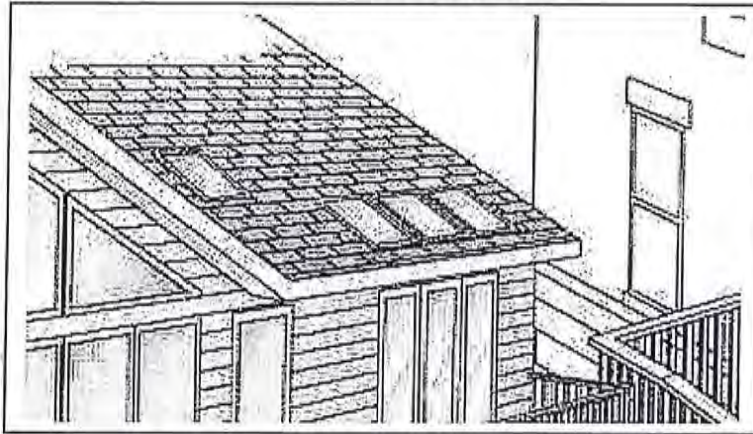
- 1) Dormers with gable, hip, or shed roofs.
- 2) Flat panel skylights or roof windows on secondary elevations.

Figure 14.134-11: Roof Elements Permitted

Gabled, shed, and hipped dormers



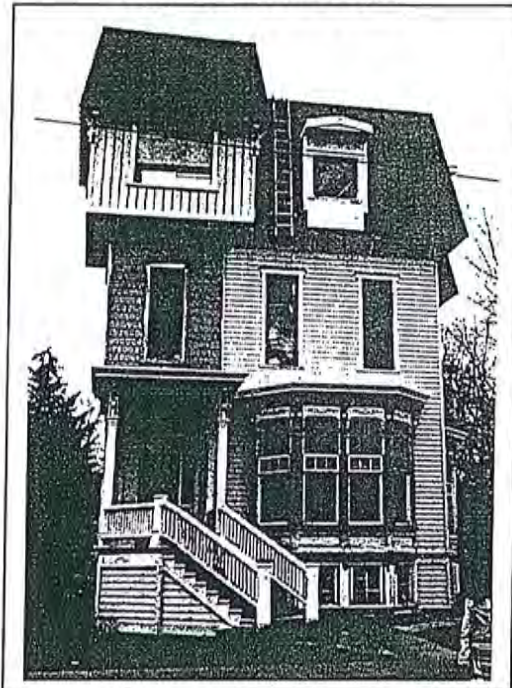
Flat panel skylights



- b. Roof elements prohibited. The following roof design elements are prohibited.
- 1) False mansard or other applied forms.
 - 2) Dome skylights.

Figure 14.134-12: Roof Elements Prohibited

False mansard roof



6. Signs.

Signs are subject to the sign provisions in Section 8.040 and 8.160.

7. Doors.

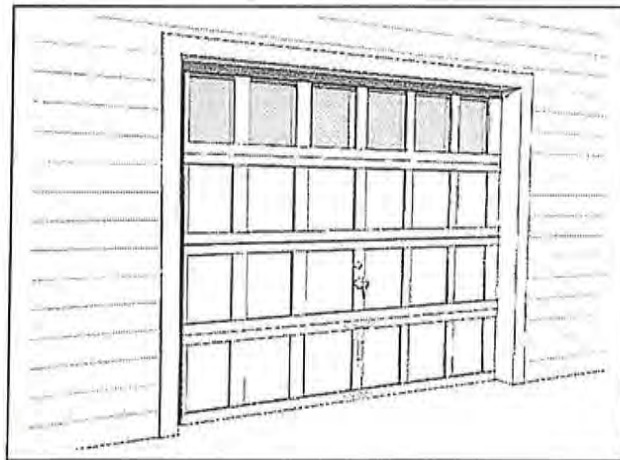
- a. Doors shall have at least one lite (glass) panel.
- b. Sliding doors are not permitted on the ground floor of the front facade.
- c. All materials are permitted.
- d. Metal or metal-clad doors shall be painted.

8. Garage Doors.

The following design standards apply to attached and detached garages:

- a. A minimum of 10% of each garage door shall be window panels, raised trim, or other architectural details.

Figure 14.134-13: Garage Doors Permitted



14.135. DESIGN GUIDELINES.

A. Purpose.

These guidelines promote architectural elements that unify the Neighborhood Greenway Area by encouraging styles characteristic of Astoria. The historic architecture of Astoria is represented by a variety of styles. Differences in details may be seen from one neighborhood to the next. These guidelines advocate the simplicity of design which is characteristic of Alderbrook and the working man's neighborhood. Building styles and details not inspired by Astoria's past will be discouraged but not prohibited. Monotony of design should be avoided. Variety of detail, form, and siting should be used to provide visual interest.

The Guidelines make reference to, but do not require the use of, historic materials. Contemporary substitutions (i.e. composite materials), will not be discouraged if their

texture, profile, and proportions are similar to those materials with historic precedent. As guidelines, the code provisions shall encourage or allow for design features or approaches that provide flexibility and discretion for the appropriate review body to interpret and apply the guidelines.

B. Design Review Process.

All uses proposed in the Neighborhood Greenway Overlay Zone other than residential single-family and two-family dwellings, or public utilities that are not located within an enclosed structure shall be reviewed in accordance with the design review guidelines of Section 14.135 pursuant to the design review procedures in Article 14.

C. Design Guidelines.

The following design guidelines shall apply to multi-family dwelling development that does not comply with the design standards of Section 14.134 and/or all non-residential development except public utilities that are not located within an enclosed structure.

1. Building Forms.

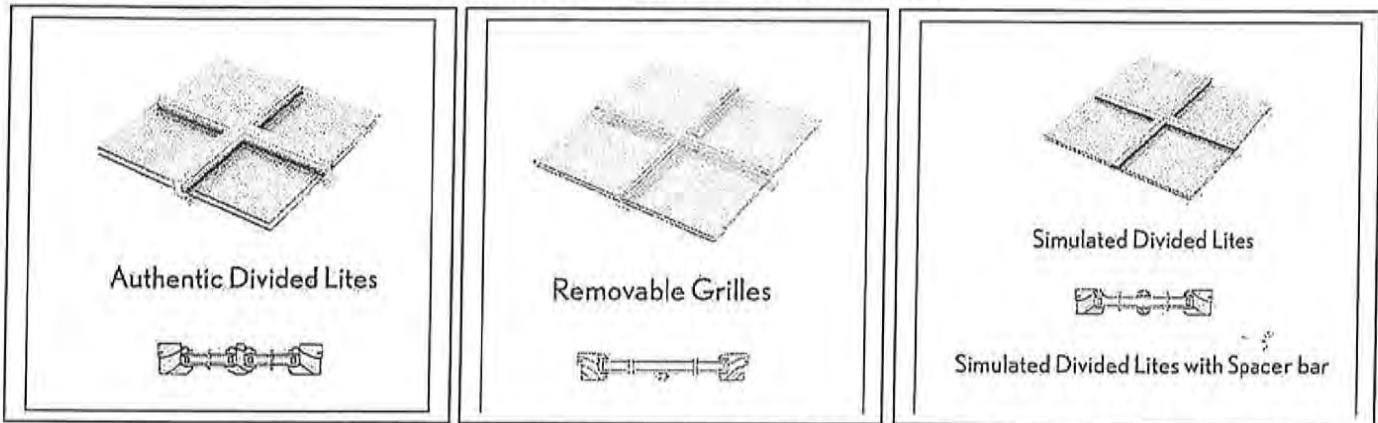
- a. All buildings should be based on a rectangular or square form.
- b. All multi-family dwelling unit buildings should have a front porch, at least six (6) feet deep and 60 square feet in area.

2. Window Design.

The following design guidelines apply to all facades.

- a. Windows required. All facades facing a right-of-way, River Trail, or common open space should have windows.
- b. Window area. Window area should be included on each level or story of the facade.
- c. Window lites. Window lite design should be one of the following:
 - 1) Single-lite windows; or
 - 2) Multiple-lite true-divided windows; or
 - 3) Combination of single and multiple-lite true-divided windows; or
 - 4) Applied muntins with profile facing window exterior.

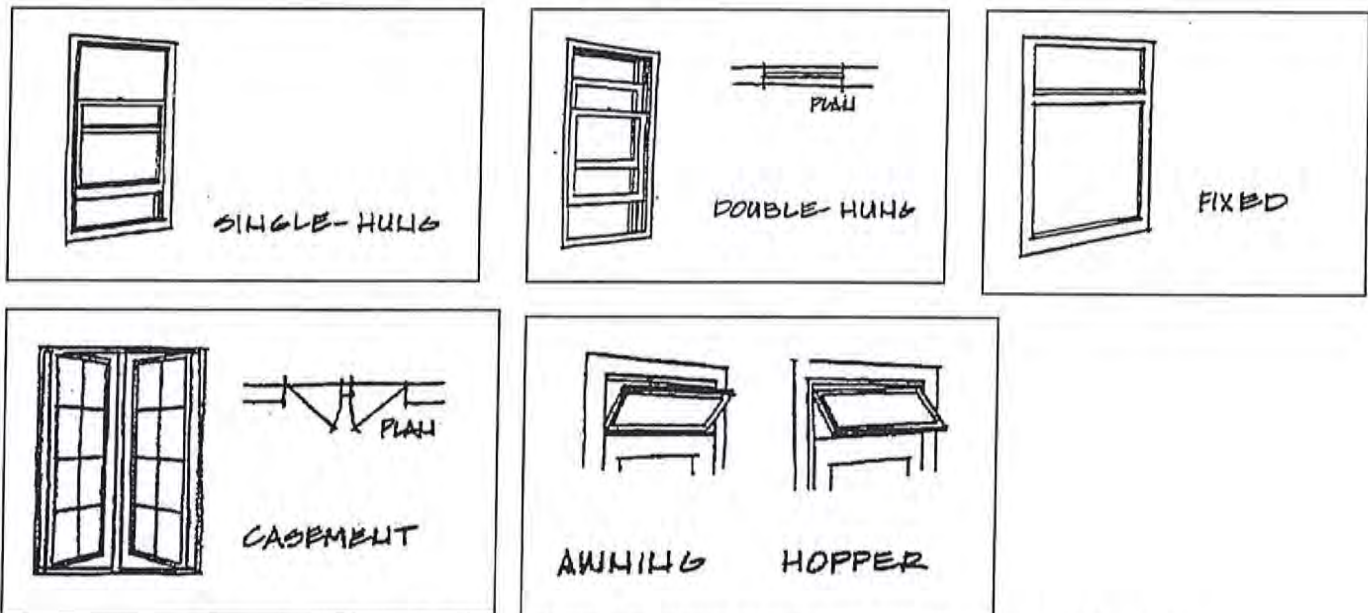
Figure 14.135-1: Window Lites



d. Windows should be fixed or open in one of the following configurations:

- 1) Fixed window; or
- 2) Single-hung windows; or
- 3) Double-hung windows; or
- 4) Awning or hopper windows; or
- 5) Casement windows.
- 6) Sliding windows may be used on non-residential structures in commercially zoned properties.

Figure 14.134-2: Fixed and Opening Windows



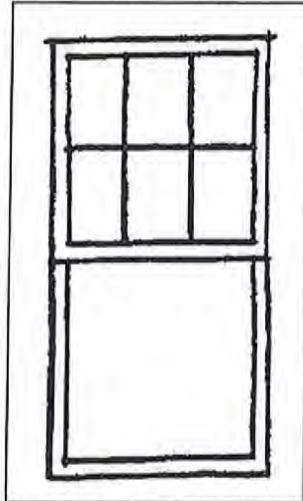
e. Window shape. Window shape should be one of the following:

- 1) Vertical rectangle; or
- 2) Square.
- 3) Arched or decorative windows are permitted but should make up a

relatively small percentage of all windows in the structure.

Figure 14.135-3: Window Shapes

Vertical rectangular window



Examples of arched or decorative windows

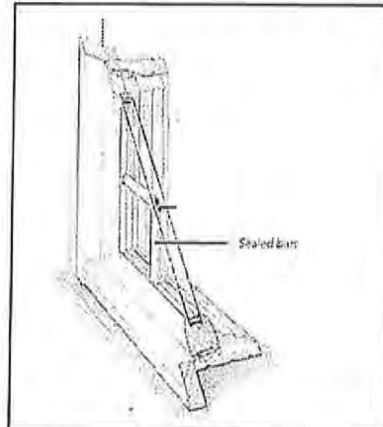


- f. Window detailing. Windows should have casings/trim, sills, and crown moldings. Casings and sills shall create a clear visible distinction and change in depth between the windows and surrounding facade.
- g. Window design discouraged. The follow window design features are discouraged.
 - 1) Applied muntins that have no profile.
 - 2) Smoked, tinted, or frosted glass, except for bathroom windows not on the street-facing facade.
 - 3) Mirrored glass.

†

Figure 14.135-4: Window Design Discouraged

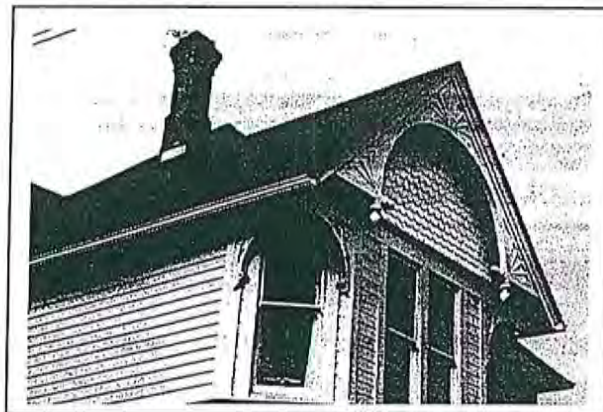
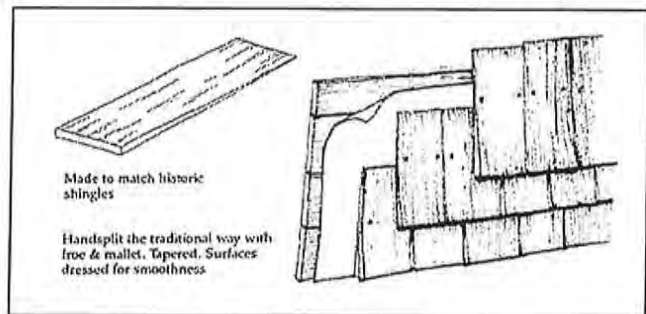
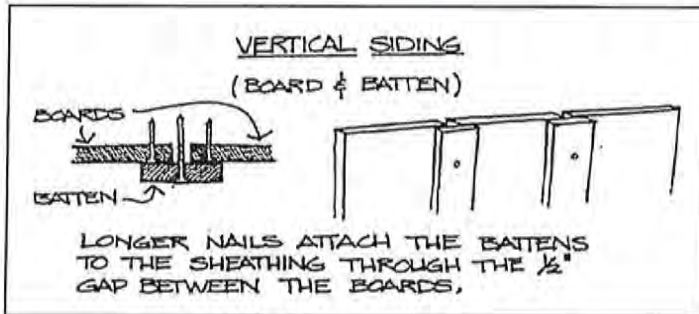
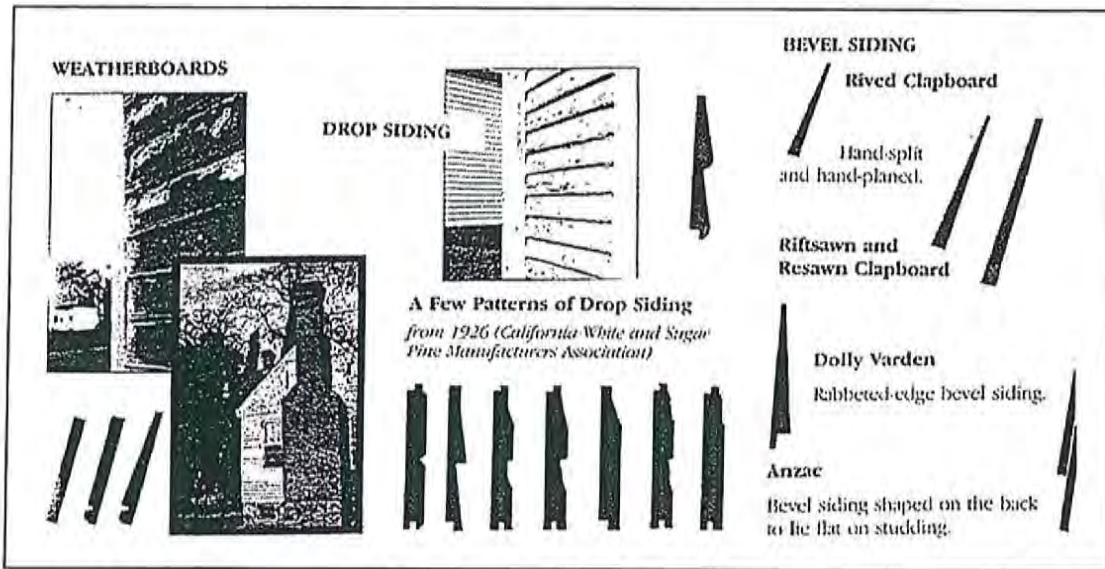
Muntins with no profile



3. Exterior Wall Treatments and Materials.

- a. Exterior walls should be constructed of one or more of the following sets of treatments and materials.
 - 1) Drop siding; or
 - 2) Weatherboard siding; or
 - 3) Clapboard; or
 - 4) Rectangular wood shingle or shake; or
 - 5) Decorative wood shingle or shake; or
 - 6) Board and batten.
 - 7) Fiber cement siding in the treatments noted above may be used but shall have a smooth, not textured, finish.
 - 8) Stucco may be used on non-residential structures in commercially zoned properties.
 - 9) Precast concrete or decorative concrete panels may be used on non-residential structures in commercially zoned properties.
- b. Horizontal siding should have six inches or less exposure.
- c. Vertical board and batten should have true battens.

Figure 14.135-5: Exterior Walls – Encouraged Materials

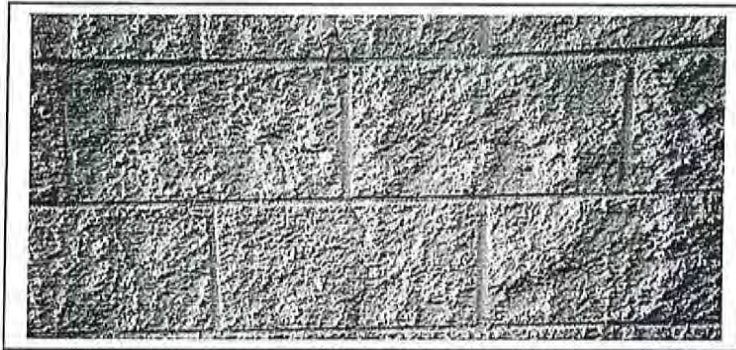


e. Exterior wall treatments and materials discouraged. The following types of treatments and materials are discouraged.

- 1) Exposed textured concrete block.
- 2) Flagstone or other applied stone products.
- 3) Precast concrete or decorative concrete panels, except on non-residential structures in commercially zoned properties.
- 4) Plywood paneling.

Figure 14.135-6: Exterior Wall Treatments and Materials Discouraged

Textured concrete



Applied stone

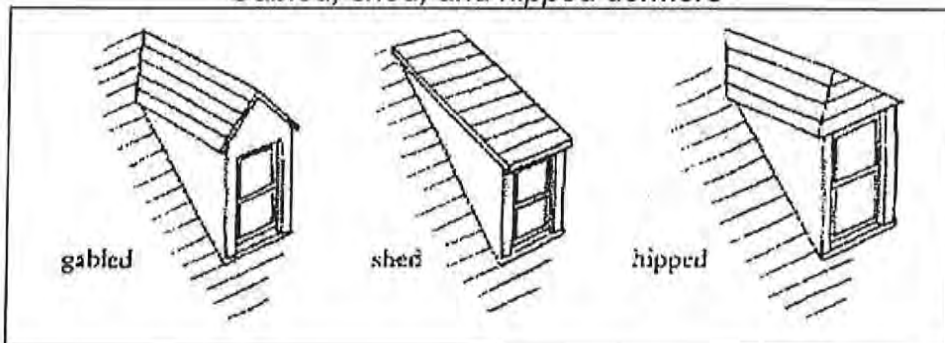


4. Roof Elements.

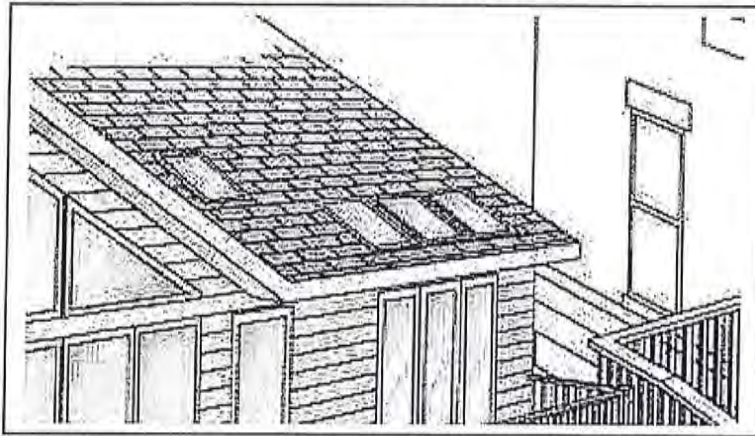
- a. Roof elements encouraged. The following roof design elements are encouraged.
 - 1) Dormers with gable, hip, or shed roofs.
 - 2) Flat panel skylights or roof windows on secondary elevations.

Figure 14.135-7: Roof Elements Encouraged

Gabled, shed, and hipped dormers



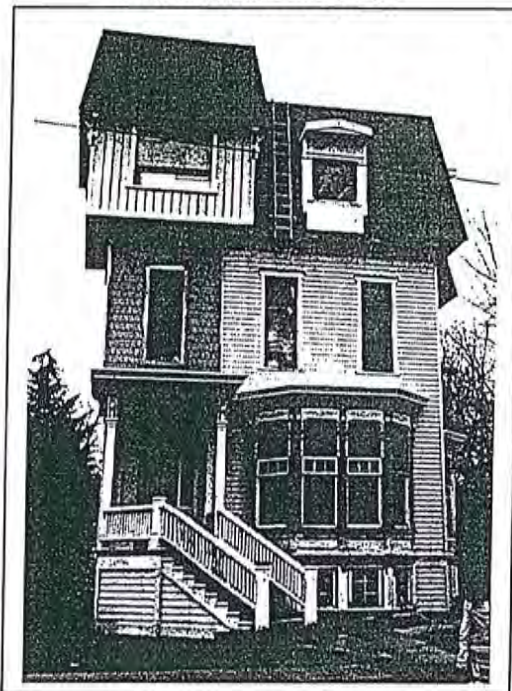
Flat panel skylights



- b. Roof elements discouraged. The following roof design elements are discouraged.
- 1) False mansard or other applied forms.
 - 2) Dome skylights.

Figure 14.135-8: Roof Elements Discouraged

False mansard roof



6. Signs.

Signs are subject to the sign provisions in Development Code Article 8.

7. Doors.

- a. Doors should have at least one lite (glass) panel except on non-residential structures in commercially zoned properties.
- b. Sliding doors are discouraged on the ground floor of the front facade except on non-residential structures in commercially zoned properties.
- c. All materials are permitted.
- d. Metal or metal-clad doors should be painted.

14.137. OTHER DEVELOPMENT STANDARDS.

A. The following development standards are applicable within the Neighborhood Greenway Overlay Zone.

1. Exterior lighting.

Outdoor lighting shall be designed and placed so as not to cast glare into adjacent properties. Light fixtures shall be designed to direct light downward and minimize the amount of light directed upward. The Community Development Director may require the shielding or removal of such lighting where it is determined that existing lighting is adversely affecting adjacent properties or contributing to light directed into the night sky.

2. Fences.

Fences located between the River Trail and the Columbia River shall not exceed a height of three (3) feet.

14.138. LANDSCAPING.

Landscaping is required in the Neighborhood Greenway Overlay Zone in accordance with the provisions in this Section and those in Section 3.120 to 3.125. The provisions in this Section apply to new construction or exterior renovations with a value of at least 20% of the assessed value of the structure, or in the event of installation of new parking areas for multi-family dwellings and/or development other than single-family or two-family dwellings. Single-family and two-family dwellings are not subject to the landscaping standards except as noted.

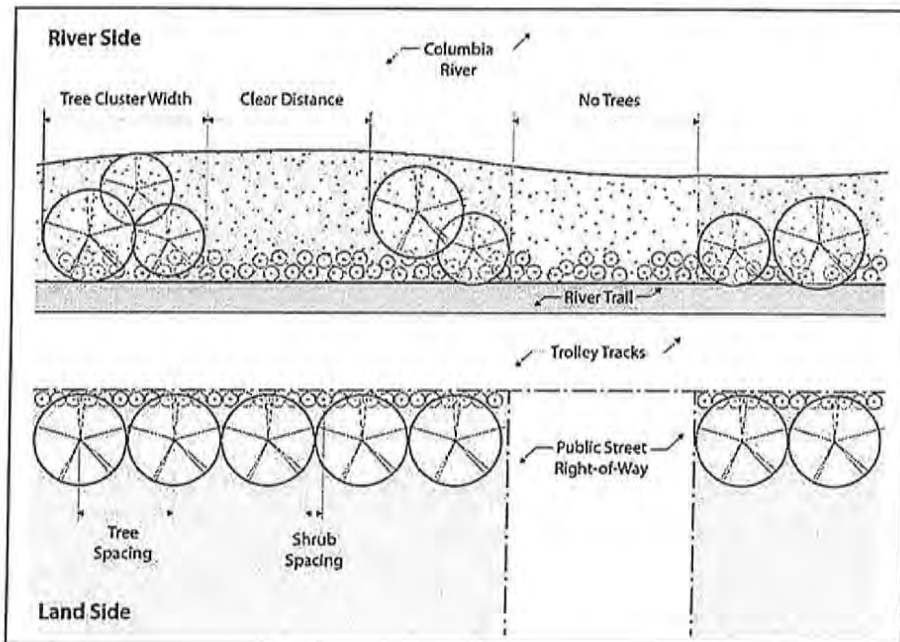
A. River Side or Riparian Standards.

1. Height and Spacing.

- a. Maximum shrub height is 30 inches.
- b. Maximum width of clusters of trees is 30 feet.

- c. Clusters of trees shall have a minimum of 50 feet clear between branches at maturity.
- d. Trees are not permitted to be planted on the river side of the River Trail within the extended public right-of-way or view corridor extending from it for a distance of 70 feet centered on the right-of-way centerline.
- e. Trees shall not exceed 25 feet in height at maturity
- f. Maximum height of fences is three (3) feet.

Figure 14.138-1: River Side/Riparian Landscaping



2. Native Plants.

See Section 3.125 concerning use of native plants and list of recommended native plants.

3. Landscaping Credits for Non-Vegetation Features.

- a. The Community Development Director may approve non-vegetative features to account for up to 40% of required landscaping when the features consist of hardscaped pedestrian-oriented areas (e.g., courtyards, plazas). Permeable paving and other stormwater management techniques are encouraged in the design of these areas.

- b. An application proposing more than 40% of required landscaping be credited by non-vegetative features is subject to approval in accordance with procedures in Article 9 and Article 12.
- c. Non-vegetative features allowed in the public right-of-way and/or on the River Trail in lieu of required landscaping shall be maintained by the applicant. There shall be a maintenance agreement or other City approved agreement. Failure to maintain or loss of the non-vegetative feature will result in the requirement for installation of the landscaping in accordance with the Code at the time of the loss.

2. Parking Area Landscaping.

- a. Landscaping required between parking areas, streets, and sidewalks in accordance with Section 3.120.A.7 shall also be required between parking areas and the River Trail.
- b. Landscaping shall minimize pedestrian exposure to parking lots with a hedge or a decorative fence that is 36" to 42" high.
- c. Maximum tree height and width in parking areas shall be 15 feet at maturity.

3. Landscaping Credits for Non-Vegetation Features.

- a. The Community Development Director may approve non-vegetative features to account for up to 25% of required landscaping when the features consist of the following:
 - (1) Hardscaped pedestrian-oriented areas (e.g., courtyards, plazas); and/or
 - (2) At least one of the following amenities meeting the City approved design within the public right-of-way and/or River Trail right-of-way:
 - (a) bike rack
 - (b) bench
 - (c) table
 - (d) drinking fountain
 - (e) directional or interpretive/information signage
 - (f) trash or recycling container
 - (g) lighting
 - (h) restroom

Permeable paving and other stormwater management techniques are encouraged in the design of these areas.

exceeding 80% of its fair market value as indicated in Section 3.190.D, provided the reconstruction of the building complies with the following standards:

1. The building shall be rebuilt on the same location on the lot, or in compliance with the setback standards for the underlying zone. This does not allow any construction beyond the property lines; and
2. The square footage of the replacement structure and/or replacement uses does not exceed the square footage of the original structure and use classifications by more than 10%; and
3. The height of the building shall be the same or less than the existing building height prior to destruction; and
4. If the property is within an area subject to architectural design review standards, the design of the replacement structure shall comply with those architectural standards; and
5. Substantial construction of the building shall begin within two years of the date of destruction, unless an extension has been granted in accordance with Section 9.100; and
6. All other City and Building Codes relative to construction, including but not limited to, geologic concerns, stormwater management, grading, driveways, sidewalks, etc. shall apply.”

Section 4. Section 3.180.D pertaining to Nonconforming Uses is added to read as follows:

“D. Reestablishment of Existing Non-Conforming Uses in Overwater Buildings

Nonconforming uses in overwater buildings located between 16th and 41st Street within the Civic Greenway Overlay Area existing prior to 2013, and between 41st and approximately 54th Street within the Neighborhood Greenway Overlay Area existing prior to 2015 may be reestablished if the building housing the use is unintentionally destroyed by any means to an extent exceeding 80% of its fair market value as indicated in Section 3.190.D, provided the reconstruction of the building complies with the standards in Section 3.190.F and reestablishment of the use occurs within one year of the completion of construction. Completion of construction shall be determined by issuance of a temporary and/or final Certificate of Occupancy from the Building Official.”

Section 5. The 1992 Astoria Land Use and Zoning Map is amended to apply the Neighborhood Greenway Overlay (NGO) Zone as indicated on the map shown as Attachment A and further described below:

The area is generally described as 41st Street to 54th Street north of Lief Erikson Drive to the pier head line.

Section 6. The 1992 Astoria Land Use and Zoning Map is amended to rezone the following area from A-3 (Aquatic Conservation) to A-4 (Aquatic Natural) as indicated on the map shown as Attachment B and further described below:

The water area generally described as 41st Street to approximately 53rd Street north of the shoreline to the pier head line.

Section 7. Section 9.100.B, Permit Extensions, is amended to read as follows:

"Permit extension may be granted for all land use permits. Extensions may also be granted for time limits applicable to non-conforming buildings and/or non-conforming uses located over water between 16th and 41st approximately 54th Streets as described in Sections 3.180.D and 3.190.F. One year extensions may be granted in accordance with the requirements of this Section as follows:"

Section 8. Astoria Development Code Section 14.015.A, Gateway Overlay Zone, General Provisions, map exhibit only is deleted in its entirety.

Section 9. Astoria Development Code Section 14.010, Definitions, is renumbered as follows:

"14.001. DEFINITION.

As used in Article 14, unless the context requires otherwise, the following words shall have the meaning indicated:

"SHOULD: A requirement, unless it can be shown that to comply with the requirement would be unreasonable, impractical, or unfeasible. Economic hardship alone shall not be justification for noncompliance with the requirement, but may be considered in conjunction with other reasons for noncompliance."

"ADJACENT: Any lot abutting the subject parcel (including all tax lots included as part of the subject parcel development) excluding rights-of-way."

Section 10. Effective Date. This ordinance and its amendment will be effective 30 days following its adoption and enactment by the City Council.

ADOPTED BY THE COMMON COUNCIL THIS ____ DAY OF _____, 2015.

APPROVED BY THE MAYOR THIS _____ DAY OF _____, 2015.

Mayor

ATTEST:

Brett Estes, City Manager

ROLL CALL ON ADOPTION:

YEA

NAY

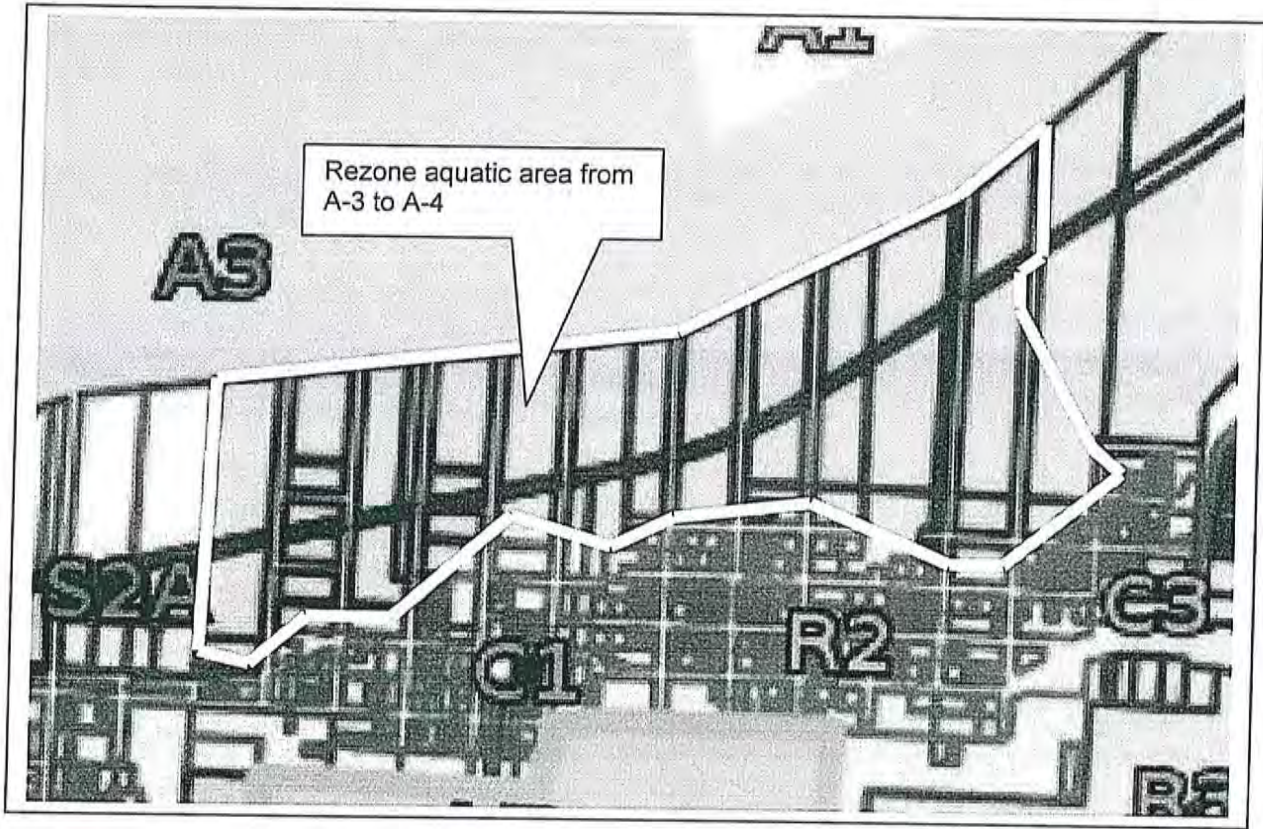
ABSENT

Commissioner Nemlowill
 Herzig
 Price
 Warr

Mayor LaMear

Ordinance 14-____ - Attachment B

A-3 (Aquatic Conservation) to A-4 (Aquatic Natural) Zone



ORDINANCE NO. 15-_____

AN ORDINANCE AMENDING THE ASTORIA COMPREHENSIVE PLAN PERTAINING TO IMPLEMENTATION OF THE ASTORIA RIVERFRONT VISION PLAN FOR THE NEIGHBORHOOD GREENWAY PLAN AREA

THE CITY OF ASTORIA DOES ORDAIN AS FOLLOWS:

Section 1. Astoria Comprehensive Plan Section CP.080, Alderbrook Area, is deleted in its entirety and hereby amended to read as follows:

"CP.080. Alderbrook Area.

The Alderbrook Area generally lies between Lief Erikson Drive and the pier head line, and from ~~42nd~~ 41st to 54th Streets. It is an area of primarily older, single-family homes, with a few scattered duplexes. The area is topographically low, and some of it is in the 100-year floodplain. There are ~~two~~ four small parks ~~-, Alderbrook Hall and Park -one on at 4509 Lief Erikson Drive; LaPlante Park in the 4500 Block Cedar Street; Birch Street Ball Field at 49th and Birch Street; and Alderbrook Lagoon Beach at the foot of 53rd Street~~ the other on Birch Street. The only commercial uses ~~are is the~~ a small business on Lief Erikson Drive, a motel at 54th and Lief Erikson Drive, and a construction business at 49th and Ash Streets. A commercial fishing facility was located on the waterfront between 49th and 50th Streets; however this facility is currently being converted into an art studio/retreat. The Area also overlaps with the Astoria Riverfront Vision Plan "Neighborhood Greenway" area which extends along the Riverfront from 41st Street to the east side of the Alderbrook neighborhood at approximately 54th Street and between Lief Erikson Drive and the pier head line of the Columbia River as depicted on the City's Zoning Map.

Alderbrook Area is the only older neighborhood directly on the waterfront, and this is discussed in the shorelands/estuary section. The area has historically been zoned Medium Density Residential (R-2). The adjacent Blue Ridge and Emerald Heights areas are zoned High Density Residential (R-3), and are primarily multi-family housing areas. The majority of structures in the Blue Ridge area were demolished prior to 2007 and the land is mostly vacant pending redevelopment. The adjacent motel is zoned General Commercial (C-3). The City limits line stops east of Blue Ridge and includes North Tongue Point industrial area and South Tongue Point, but excludes North Tongue Point Job Corps Center and the US Coast Guard station, although this area is in the City's Urban Growth Boundary.

Advantages of the Alderbrook Area are the proximity to the waterfront, the lack of through traffic (away from Lief Erikson Drive), the neighborhood hall and neighborhood character, and the availability of lower cost housing. However, the limited access into the Alderbrook Area from Lief Erikson Drive creates concerns with increased traffic to the area with no secondary vehicular outlets. Disadvantages include the flooding potential, the traffic along Lief Erikson Drive (a problem when crossing to the playground), and the distance to school."

Section 2. Astoria Comprehensive Plan Section CP.085.2, Alderbrook Area Policies, is deleted in its entirety and hereby amended to read as follows:

"The residential character of Alderbrook will be protected through the designation of the aquatic area from 41st Street to Tongue Point as ~~conservation~~natural, and by the present zoning pattern. Development in the 100-year flood area shall be subject to the requirements of the City's Flood Hazard Overlay Zone."

Section 3. Astoria Comprehensive Plan Section CP.085.3, Alderbrook Area Policies, is deleted in its entirety and hereby amended to read as follows:

"3. ~~Light industrial~~water-dependent / water-related development consistent with the ~~conservation~~natural estuary designation (such as commercial fishing operation) may be allowed so long as it does not conflict with the residential area and is consistent with the City's Riverfront Vision Plan."

Section 4. Astoria Comprehensive Plan Section CP.085.6, Alderbrook Area Policies, is deleted in its entirety and hereby amended to read as follows:

"6. The property west of the sewer lagoons is ~~presently~~ designated as a ~~potential~~ park site and is the current (2015) east terminus of the River Trail; it is zoned Institutional (IN) Medium Density Residential (R-2). However, the site's use as a regional park raises problems of traffic generation on residential streets. The local community must be involved in any future decision regarding this area."

Section 5. Astoria Comprehensive Plan Section CP.085.8, Alderbrook Area Policies, is hereby amended by the addition to read as follows:

"8. Extension of the trolley service to the Alderbrook neighborhood should be investigated and considered.

Section 6. Astoria Comprehensive Plan Section CP.175.F, Uppertown / Alderbrook Subarea Plan, Aquatic and Shoreland Designations, is hereby deleted in its entirety and hereby amended to read as follows:

"The aquatic area between 29th and 41st Streets is designated Development to the pierhead line, except at the East End Mooring Basin where the designation corresponds to the outer boundary of the pier. East of 41st Street, the aquatic area is designated ~~Conservation~~Natural."

Section 7. Astoria Comprehensive Plan Section CP.150.B, Permitted Uses in Columbia River Estuary Aquatic and Shoreland Designations, Natural Aquatic, is hereby amended by the addition to read as follows:

"20. In pile supported buildings existing as of October 1, 2002, non-water dependent and non-water related uses."

Section 8. Effective Date. This ordinance and its amendment will be effective 30 days following its adoption and enactment by the City Council.

ASTORIA PLANNING COMMISSION WORK SESSION

Astoria City Hall
September 1, 2015

CALL TO ORDER:

President Pearson called the meeting to order at 6:30 pm.

ROLL CALL:

Commissioners Present: President David Pearson, Vice President McLaren Innes, Kent Easom, Sean Fitzpatrick, Daryl Moore, Jan Mitchell and Frank Spence

Staff Present: Community Development Director Kevin Cronin, Special Projects Planner Rosemary Johnson, and Consultant Matt Hastie, Angelo Planning Group. The meeting is recorded and will be transcribed by ABC Transcription Services, Inc.

NEW BUSINESS:

Item 3(a): Riverfront Vision Plan – Neighborhood Greenway Area

Planner Johnson explained this was the first work session to discuss the Neighborhood Greenway Area of the Riverfront Vision Plan. She gave a brief overview of the Plan, describing the location and boundaries of the Neighborhood Greenway area, and noted that the Plan was now being implemented into the City's Development Code and Comprehensive Plan. This work session would include an overview of the Code amendment process, as well as a review of the goals, the issues based on the goals, the Town Hall meeting held on August 20, 2015, and key issues and options for the Neighborhood Greenway Area. After input from the public and Planning Commission, staff will take guidance from the Commission on several options.

Matt Hastie, Angelo Planning Group, said several things, such as transportation projects, are necessary to implement the Riverfront Vision Plan. However, the Planning Commission would only focus on the elements of the Plan that could be implemented through the Development Code. He reviewed the goals and objectives identified for the Neighborhood Greenway Area in the Plan. He and Planner Johnson gave a PowerPoint presentation on the preliminary recommendations for the Code amendments necessary to implement the Plan in a way that would meet those goals and objectives. During the presentation, Staff posed the following questions for the Commission to consider:

1. Should residential accessory structures larger than a 1-car garage (14-feet by 20-feet, 280 square feet) require review or be allowed with no design review?
2. Should design guidelines, standards, or a combination of guidelines and standards be implemented? Design guidelines would have to be reviewed and approved by the Design Review Committee. Standards could be approved administratively if all standards were being met.
3. Should design guidelines and/or standards be implemented for non-residential structures? Standards would require a strict but simple process. Guidelines would allow more flexibility, but require the design review process. If both were implemented, a review would be required when standards are not met. Should the Code include standards and/or guidelines for roof designs?

Staff detailed next steps as follows:

- Continue to receive public feedback.
- Revise the draft Code amendments based on feedback from the public and the Planning Commission.
- Discuss revisions at the Planning Commission Work Session scheduled for September 16, 2015.
- Update amendments based on direction from the Commission given at the next work session.
- Schedule public hearings with the Planning Commission and City Council in October and November.

President Pearson invited public comments.

Michael Zarnetkin, 373 47th Street, Astoria, said everyone who was at the meeting attended because they love Alderbrook. The neighborhood has been described as having a small town charm and quirky vibe. He chose to live in Alderbrook for its affordability and location, which allowed him to create pottery, pursue his craft, and raise

his family without intrusive government interventions. He has lived in Alderbrook for more than 40 years and does not plan to move. Most of the people in the neighborhood mind their own business and help each other when they can. Each person has a right to their own way and individual pursuit of their own happiness. Alderbrook has been allowed to develop relatively unmolested from unnecessary rules, regulations, zoning, land use ordinances, and arbitrary City officials. The zoning does not need to be changed from A-3 to A-4 and the neighborhood does not need accessory structure ordinances or multi-family development standards. Imposing design standards that regulate the type, shape, and style of windows, and regulate roofs and exterior siding are excessive interventions into an individual's property rights. Alderbrook has become what it is today because these rules and ordinances never existed. The neighborhood has a live and let live spirit. Building codes are necessary and important because individual and public safety is involved. However, design codes have nothing to do with safety and everything to do with one's personal aesthetic taste. Taste is individual, in the eye of the beholder. He believed each person had a right to use their own property as they choose, as long as building safety codes and general zoning ordinances are respected.

Mike Abrams, 4880 Ash Street, Astoria, said he is the third generation of his family to live in Alderbrook. He asked the City to just leave the neighborhood alone because it is fine the way it is. The waterfront area is a tidal marsh zone, which goes dry twice a day, so no marina could be built. None of the old fishing docks and boat ways are left along the working waterfront because Mother Nature has destroyed them. The residents love the area. In the early 1990s, the neighborhood was rezoned from Marine Industrial to a neighborhood zone, so the working waterfront was taken away. Oregon owns everything under the water, so any overwater development will owe rent. There are already many rules in place for the area. He asked that Alderbrook be left alone.

Ken Adee, 5275 Ash Street, Astoria, said he agreed with Mr. Zametkin and Mr. Abrams. He wanted the area to be kept the way it is. He believed there was consensus at the Town Hall meeting of residents that like Alderbrook the way it is and do not want it to change. He requested the area between the sewage lagoon and the Alderbrook lagoon be rezoned from Institutional to S-5, Natural Shoreline. He believed the S-5 zone was more in keeping with the use of the property. He spends a lot of time in this area of the River Walk and has spoken to many people who say they go to this area because it is natural, peaceful, has a large green area, provides bird watching, and access to the water. These uses are important in this area. He was most concerned with the waterfront and natural areas of Alderbrook and has been working with the City and watershed association to reduce invasive plants and reintroduce native plants. Any docks built over the water would not be useful because the marsh is dry most of the time. He asked the Planning Commission to keep the area the way it is.

Martin Pollard, 5323 Ash Street, Astoria, believed Staff said at the Town Hall meeting that no changes would be made to the A-3 zone because the zoning protected the natural area. Now, Staff is saying the zone might be changed to A-4, which means uses will be more restricted. He questioned what Alderbrook had to do with the River Walk, noting that the two areas are about a ½ mile apart. He did not believe carnival rides or hot dog stands would be developed and wanted to know the purpose behind the proposed developmental changes to Alderbrook compared to the function of the River Walk. The River Walk is a pleasant place that is well maintained for walking. He asked what the purpose or vision was for the green area at the end of the River Walk. He believed the Institutional zone was part of a land grant and had limited uses. It is obvious that Alderbrook residents are happy and content with what they have, which includes multiple cultural structures. Therefore, the City cannot make a code that requires a certain roof pitch or material. The recommendations state there should not be any changes to residential development. However, he was concerned about the developmental issues and asked what was being developed and where. These Code amendments are being made for a reason, but the residents do not see the reason. He wanted to know what development would be coming to the neighborhood in the future. The neighborhood is not seeing the reason for all of these changes. The River Walk is a ½ mile from Alderbrook and he wanted to know why the City believed the River Walk impacted Alderbrook so substantially at this point.

Josie Peper, 5276 Ash Street, Astoria, believed the City should be more protective of the Institutional zoned area by changing the zoning to something more conservative. She also believed the City should figure out what to call the area. Director Cosby referred to it as the Astoria Natural Area, Planner Johnson referred to it as the Alderbrook Lagoon Park or Beach, some people call it Stinky Beach, and others call it Pretty Beach. The zoning should be changed to something that goes along with the wording in the Comprehensive Plan and Riverfront Vision Plan.

Staff responded to public comments about the Institutional Zone and the S-5, Natural Shoreland zone. The Institutional Zone includes the college campus, City facilities, and parks. The public uses allowed within the zone

have slightly different and more flexible standards than the public/semi-public uses allowed in residential zones, which require a conditional use permit. Staff had no intentions of relocating or expanding the sewage treatment plant and the park would remain a park. The recommended Code amendments are for the approximate 22 parcels that could be redeveloped. The City does not have any plans to do anything different with the Institutional Zone. Staff would need to review the uses allowed in the S-5, Natural Shoreland zone because some of the desired amenities for this area, like a walking trail, bridges, or restrooms, would be included in the Parks Master Plan.

Josie Peper, 5276 Ash Street, Astoria, understood the City had previously proposed to install windmills in the area. She was concerned about development like windmills or a dog park and asked if there was a way to create an overlay zone to further restrict uses.

Planner Johnson explained the Neighborhood Greenway Area is an overlay that would apply to the Institutional Zone, which includes the park.

Commissioner Easom said the entire area between 39th and 45th Streets appeared to be undeveloped and he wanted to know who owned the property.

Planner Johnson said properties from 39th to 41st Street include condominiums and the Astoria Business Park. The western edge of Alderbrook Lagoon includes a City-owned parcel and part of the River Trail. At about 43rd Street and up to 45th Street, there are a few houses on the south side of the river. She used a map to show the exact locations of each property in the area, noting that the City owns most of the rights to the areas over the water. She referred to Page 5 of the agenda packet, which showed parcels owned by the City and the Division of State Lands. Upland property owners have the first rights of refusal to use the parcels owned by the Division of State Lands. Very few overwater properties in this area are owned privately. She noted the condominiums and business park are within the Civic Greenway Area, as the Neighborhood Greenway Area only extends to 41st Street on its western edge.

In response to a question about whether these code amendments were necessary for the City to move forward with some planned developments, Mr. Hastie confirmed nothing had been planned. The City does not intend to develop any structures in the area. These Code amendments are necessary to implement the Riverfront Vision Plan. Current standards for overwater development in this area allow 20-foot tall structures in the A-3 zones. The recommendations in the Plan suggest open views and vistas should be maintained. Therefore, Staff is recommending additional future buildings not be allowed over the water unless they are bank height or lower. Recommended landscaping provisions are also intended to keep areas open, maintain views of green areas, and keep the area natural. The Plan indicated people like the area just the way it is, so Staff has refrained from recommending any new standards for most of the existing development. Standards recommended for larger or nonresidential development are intended to maintain the character of the neighborhood. He believed most of the recommendations would maintain the area as is.

In response to the comment about the River Trail being a ½ mile from Alderbrook, Mr. Hastie explained that the Riverfront Vision planning process from 2005 to 2007 considered the entire area between the highway and the pier head line, which includes areas over the water, the River Trail, and the residential neighborhood.

Vice President Innes reiterated that the design standards would only apply to windows and roofs on new non-residential structures of a certain size. Planner Johnson added the structures would include buildings like churches, schools, and daycare centers. Staff is not suggesting any design review of residential structures and the standards for accessory structures are very basic.

Commissioner Easom asked what prompted the recommendations for accessory structures. Planner Johnson explained that several years ago, the City received a request for a garage/pole barn in Alderbrook. The accessory structure would be larger than the residential structure. Neighbors tried to work out the issues amongst themselves, as is typical in Alderbrook. However, the property owner built the pole barn, which he had the right to do. Residents in Alderbrook did not want new restrictions on their properties, but they did not want large pole barns in the neighborhood either. Staff reviewed possible Code amendments to address the size and scale of accessory structure, but chose not to pursue implementation at that time. Staff is proposing these standards now to protect the character of Alderbrook from intrusive and large development. However, the Riverfront Vision Plan does not specifically address accessory structures.

Commissioner Mitchell said the visioning process does not help the City at all unless the zoning is changed. She believed this concept was difficult to understand. People like the views as they see them today and assume it will continue because everyone agrees they enjoy the views. However, a developer can build whatever the zoning allows. Staff cannot refer to the Riverfront Vision Plan just because it says nice things. This was an issue during implementation of the Bridge Vista Area. She was sure many people believed the City had sold them out. However, the City actually removed much of what had been allowed. It can be very difficult to understand that sometimes, Code amendments are necessary to prevent things from happening. Alderbrook is a wonderful neighborhood with a distinctive character. The traffic needs to slow down and the City should consider a way to prevent people from getting lost in the neighborhood. These recommendations will put in place the codes necessary to retain the character of the neighborhood and protect the neighborhood from inappropriate development.

Mr. Hastie agreed and said the intent is to restrict what could occur and maintain the existing character, more so in this area than in any other area along the riverfront. Staff is trying to limit the types of uses that would have further regulation. Most of the area consists of single-family and two-family homes and Staff is not recommending additional regulations to those uses.

The Planning Commission discussed the proposed Code amendments and Staff's questions. They provided feedback and responded to Staff's questions with the following key comments:

- Change the A-3 zone to A-4, as recommended.
- No overwater standards were necessary; the A-4 zone with the top of bank height limitation would limit height and width of structures.
- After some discussion of the current standards and uses for accessory structures, the Commission decided to eliminate proposed Code amendments for accessory structures. Staff will discuss residential use of accessory structures as they relate to the housing study with City Council on September 14.
- Commissioners agreed that non-residential structure designs should be reviewed after discussing design review in other R-2 zones in the City.
 - Staff noted that State law requires clear and objective standards for multi-family housing, but alternative design guidelines with a review could be offered as well. Staff explained how standards are reviewed differently from guidelines, noting that the Commission could implement both, and discussed standards versus guidelines in other areas of the City.
 - Commissioner Easom wanted to hear from the public. Staff, Commissioners, and the audience discussed current regulations for multi-family structures. President Pearson explained the design review process, confirming it was thorough.
 - The Commission decided to implement guidelines for non-residential structures. Multi-family structures should have standards, per State law, and guidelines as an option.
- On non-residential structures, roofing materials should be included in the guidelines and standards, and roof pitch should be similar to adjacent properties.
- Commissioners had no comments on the proposed landscaping guidelines for future development.

President Pearson confirmed there would be no more public comments at this meeting. However, there would be an opportunity for the public to give comments at the next public hearing.

Staff confirmed the Town Hall meeting, public comments at this meeting, and feedback from Commissioners resulted in clear direction for revising the Code amendments. Revisions would be presented in a work session at the end of the Planning Commission meeting on September 16, 2015, which starts at 6:30 pm. The public hearing is scheduled for October 27, 2015. Staff will continue to accept public comments throughout the process, but the hearing in October will be the last opportunity for the public to give the Planning Commission input.

ADJOURNMENT:

There being no further business, the meeting was adjourned at 8:04 pm.

APPROVED:

Community Development Director

ADJOURNMENT TO WORK SESSION:

Riverfront Vision Plan – Neighborhood Greenway Area

Planner Johnson gave a brief overview of the changes made to the recommendations since the last work session. She noted the changes, which were based on input from the public and Planning Commission and public, were as follows:

- Eliminated all references to residential accessory structures.
- Established clear and objective design review standards for multi-family dwellings.
- Established design guidelines similar to the Gateway Master Plan Area for multi-family dwellings and non-residential structures.
- Included roof design and materials in the standards and guidelines.

She noted the area contains very few lots that could be developed. However, some lots could have multi-family, commercial, semi-public, or public facilities. Staff is still fine-tuning the code language to ensure all of the recommendations meet legal requirements for guidelines and standards. The final draft will be presented to the Planning Commission in a public hearing on October 27, 2015, and then to City Council in a public hearing in November.

Director Cronin added that the City did not have any plans to make changes to the sewage treatment plant or City property. He has requested demographic information about the area to understand how residents use the parks in the area. However, Staff has not had time yet to collect that information.

Planner Johnson made maps available that show which lots are currently vacant and could be developed.

Commissioner Eason believed Staff's changes were complete and accurate. Vice President Innes and President Pearson agreed.

Commissioner Moore referred to Page 4, Design Standards B.1(a) noting that squares are rectangles. He asked if there was something more to the standard that he did not understand. Planner Johnson explained the wording used was typical language. A square and rectangle have slight differences and the Code is written for the common person to understand.

President Pearson invited public comments.

Josie Peper, 5276 Ash Street, Astoria, said the draft states the aquatic area east of 41st Street has been designated Natural instead of Conservation. She asked if the zone, which is A-4, should be added in parenthesis. Planner Johnson explained Ms. Peper was referring to the Comprehensive Plan amendment on the last page of the ordinance document. The Comprehensive Plan just describes areas and does not assign zone numbers. Zoning designations are part of the Development Code. The Ordinance that amends the Comprehensive Plan therefore does state A-4 in parenthesis.

Ms. Peper asked why the Institutional, IN zone would not be changed, specifically the Alderbrook Beach area. Planner Johnson said the Institutional zone allows park facilities. Staff considered the Shoreland Natural, S-5 zone, but that zone would not allow all of the park uses. If any changes were made, a new Park Overlay Zone would need to be implemented. The Parks and Recreation Department is in the process of creating a master plan, which will consider the need for any overlay zones or additional restrictions for park areas. At this time, the Institutional zone allows the best uses for the beach area.

Director Cronin added that the Community Development Department will work with the Parks Department on the Parks Master Plan. When the master planning process reaches a certain point, Staff will make a presentation to the Planning Commission and gather input from the public.

There being no further business, the meeting was adjourned at 9:03 pm.

APPROVED:

Community Development Director

Public Comment Summary

Town Hall, August 20, 2015

Astoria Riverfront Vision Code Assistance

Phase 3: Neighborhood Greenway Area

Introduction/Overview

This report provides a summary of public comments received related to the Town Hall meeting held on Thursday, August 20, 2015, at the beginning of Phase 3 for the Neighborhood Greenway area. The report categorizes the comments according to topic areas and, in some cases, summarizes responses to the comments.

Public comments came from the following sources:

- Verbal comments made by participants during the meeting
- Comment sheets that were provided at the meeting and then were completed and returned by participants
- Emails sent to City staff following the meeting

Notes of the verbal comments, completed comment sheets, and emails sent to the City are included with this summary as Attachment A.

Comment Summary

Wayfinding, Signs, and Maps

- Provide signs for guiding people through neighborhood and to the River Trail. (*City staff response: The City is pursuing grants for wayfinding signs.*)
- Provide a map at the Alderbrook Park trailhead.

River Trail

- Keep trails on public land.
- Who owns the trail/trestle? Are there plans to modify the trestle to allow for bigger boats? (*City staff response: As a Rails to Trails project, the City is interim owner, not permanent owner. There are no plans to modify the trestle.*)
- The new paved trail is appreciated.

Landscaping

- Private property should not be further restricted in the area, particularly additional landscaping regulations. Issues like landscaping should be worked out and reflect common respect between neighbors.
- Can trees be removed in the Neighborhood Greenway area? There are particular concerns about dead trees near 45th/46th and Cedar. (*City staff response: Contact the City about regulations and procedures related to tree removal.*)

Development Restrictions

- Alderbrook is valued as it is, and either no change in regulations or no more restrictive regulations are wanted to preserve it.
- Can overwater/in-water uses be restricted in the area, e.g., dredging, private docks, covered marinas? *(City staff response: Uses could be further regulated by rezoning to another more restrictive City zone (e.g., A-4 zone) or by adding regulations related to uses that are specific to the Neighborhood Greenway area.)*
- Could the existing IN zoning in the area (Alderbrook Park) be changed to a more restrictive conservation zone?
- Are concentrations of uses regulated? *(City staff response: To an extent, existing development regulations limit the density of a use or uses on a site and conditional use criteria require a proposed use to demonstrate how it is compatible with surrounding uses including the cumulative impact of development.)*
- Is development permitted and, if so, can this development be limited on private overwater parcels? *(City staff response: Very few uses are permitted in the overwater (A-3) zone in this area, and these are primarily river-oriented or water-dependent uses. The few parcels with private rights of first refusal are mapped (shown on a poster on display at the meeting). Limits such as restricting height of this development to top-of-bank can be considered.)*
- Is height restricted in the neighborhood? *(City staff response: Buildings may generally be a maximum of 28 feet in the R-2 zone under current regulations. Other height restrictions can be considered as part of this project.)*
- There are concerns about large pole barns and garages. *(City staff response: Restrictions can be considered for these accessory structures.)*
- Would new development regulations be retroactive? *(City staff response: New regulations would not generally be retroactive. However, they would apply to redevelopment or rebuilding on a property, except as otherwise specified.)*

Park Facilities

- It is preferred that Alderbrook Park remain as a natural, passive recreation space. *(City staff response: This can also be addressed through the upcoming Parks Master Plan process.)*
- Can restrooms be provided near LaPlante Park? If so, regulate hours of use. Consider water-less facilities like those at Fort Clatsop. Also do not use port-a-potties with doors that slam shut.
- There are concerns that providing restrooms at parks in the neighborhood will draw even more people, traffic, and noise. However, there are also concerns with inappropriate use of grounds in lieu of use of restrooms.
- There are concerns about park security. Limit hours of operation of/access to parks. *(City staff response: Park hours are currently 6:00 a.m. to dusk. Other park regulations can be considered during the upcoming Parks Master Plan process.)*
- Restrict noise in the parks (e.g., motorized bikes, drones and model aircraft). *(City staff response: There are existing municipal code provisions that can be enforced to help address this.)*
- Do not use the name “Stinky Beach.”

Other Issues

- Identify City property on future maps.
- Are there (deed) restrictions on land owned by/leased to the City in the Neighborhood Greenway area? Does the City have plans to develop the land? *(City staff response: There are currently no plans to develop City land. If proposals arise in the future, those proposals would need to go through a public review process.)*
- What are the historical connections to the river in the area? *(City staff and participant response: There was fishing industry (e.g., fish net repair loft) and a lumber mill in Alderbrook.)*

Issues Outside Project Area and Scope

- There are concerns about stream area east of the lagoon being developed, especially given concerns about protecting salmon and steep hillsides (the possibility for landslides) in the area. *(City staff response: This area (Blue Ridge) is outside the project area. However, any development permitted under current zoning in the area*

would need to provide geotechnical and other environmental reporting to demonstrate that building could occur safely and could mitigate impacts.)

- Regulate jet skis in Alderbrook Lagoon (*Participant response: Natural conditions, including lagoon water levels, should limit the use of motorized water craft.*)
- There are concerns about people camping in Alderbrook, trash, and not feeling safe. (*City staff response: Regulations are now stricter about clearing camps. Homelessness and camping are complex issues faced by cities nationwide. The City has formed a task force to address homelessness in Astoria.*)
- There are concerns about tent caterpillars along the River Trail. (*City staff response: These concerns are being forwarded to the Parks Director and to the Columbia River Estuary Study Task Force.*)
- Do not allow Fire Department use Alderbrook Park for fire practice/training.
- There are concerns about the dry dunes vegetation and fire hazard.

Transportation and Traffic

- Extend trolley to LaPlante Park.
- There are concerns about additional traffic through the area, including vehicle traffic and pedestrian traffic if trails provide access to Tongue Point. There is high-speed, loud traffic in particular near the end of 54th Street, late at night.
- Control types and sizes of vehicles (larger vehicles such as tour buses) in neighborhood.
- Can speed limit signs be installed in the neighborhood? (*City staff response: The City typically does not post signs on local streets where it should be understood that the speed limit is 25 mph. Posting signs can be considered.*)
- The intersection at Crest Motel feels unsafe, arrows directing turns are faded. More clearly divide traffic in the driveway. (*City staff response: Refer to Transportation System Plan (TSP) in hard copy here at the meeting or on the City's website to see whether improvements for that intersection are planned. Engineering Department staff will look into it further.*)
- Can a light be installed at 45th Street? (*City staff response: Check the TSP for a project at that location. It is a State highway under the jurisdiction of ODOT.*)
- Will the Bypass be built? (*City staff response: The Bypass is recommended in long-range plans but is expensive, will require a lot of process and permitting, and is currently not expected to be funded and built in the next 20 years. However, an alternative route through the Clatsop Forest is being investigated for emergency use.*)

Attachment A:
Public Comment Notes, Comment Forms, and
Emails

Rosemary Johnson

From: Rosemary Johnson
Sent: Friday, August 21, 2015 1:21 PM
To: 'Kit Ketcham'
Cc: Kevin Cronin; Sherri Williams; Angela Cosby; rcrater@columbiaestuary.org
Subject: RE: tent caterpillars along the Riverwalk

Thanks for your comments. I am forwarding your question on the caterpillars to our Parks Director to see if she has dealt with this yet. Since it is also along the estuary, I'm forwarding it to CREST to see if they have any input on what to do. We will add your email to the comments from last night. Hope to see you at future work sessions on this issue. Rosemary

Rosemary Johnson, Special Projects Planner
 City of Astoria
 1095 Duane Street
 Astoria OR 97103
 503-338-5183
rjohnson@astoria.or.us
www.astoria.or.us

From: Kit Ketcham [<mailto:lilyloosy4@gmail.com>]
Sent: Friday, August 21, 2015 10:47 AM
To: Rosemary Johnson
Subject: tent caterpillars along the Riverwalk

Dear Rosemary (the Wonder Woman of Astoria's Special Projects!),

One compliment and one thing I forgot to bring up at last night's meeting:

You definitely are a wonder! You did a great job last night helping us sort through the many issues potentially affecting Astoria's beloved Alderbrook neighborhood. Thanks for your encyclopedic knowledge of the background and legal matters/ordinances that are part of the situation. You were skilled at repeating people's comments accurately---really important! The meeting was informative and helpful for this fairly new Alderbrook resident.

What I would like to add to the commentary is a question: is there a plan for taking steps to control the tent caterpillars that have begun to infest the alders along the path? I moved to the North Coast from Whidbey Island, which had a huge problem with tent caterpillars several times while I was there. I'm not sure of the best method for dealing with them; they're mostly ugly, I think, but can damage trees irreparably under some circumstances.

Anyhow, just wanted to throw in another two cents worth and offer kudos on how you handled the meeting last night. Thanks.

Kit Ketcham (who now lives at 5360 Alder St., just steps from the Riverwalk entrance.)

--

Kit Ketcham
 Minister Emerita, Unitarian Universalist Congregation of Whidbey Island



CITY OF ASTORIA
Founded 1811 • Incorporated 1856

To Shayna 8/20/15

Riverfront Vision Plan Implementation Neighborhood Greenway Area Town Hall Meeting 8/20/15

Comment Form

Your comments will be considered in updating the City's development code to implement the Riverfront Vision Plan for properties in the Neighborhood Greenway area.

Leave Alderbrook alone thank you
Limitations on private property development.
I had a view of the RIVER for 23yrs
til my new neighbor in front of me corner
47th & Birch built a huge Shop. The City
allowed this without neighbors permission.
Keeping the land as natural as possible
guarding the wildlife, birds fish etc

Optional - Name & email: Cheryl Kiwi-chick 49@yahoo.com
Address: _____

Thank you for your comments! Please leave this completed comment form with one of the project team members before you leave the meeting, or return it to:
City of Astoria, Community Development Department, 1095 Duane Street, Astoria OR 97103;
or email comments to rjohnson@astoria.or.us.



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Riverfront Vision Plan Implementation Neighborhood Greenway Area Town Hall Meeting 8/20/15

Comment Form

Your comments will be considered in updating the City's development code to implement the Riverfront Vision Plan for properties in the Neighborhood Greenway area.

We really need a park^{w/ restrooms} or just restroom facility @ LaPlante
in the alderbrook neighborhood. or at least
on the East end of the River walk.
Don't have to have water —
Like the ones at Ft Clatsop would
be a definite. But
Please, no Porta Pottys.

Optional - Name & email: Jennifer Parsons

Address: 365 53rd ST

Thank you for your comments! Please leave this completed comment form with one of the project team members before you leave the meeting, or return it to:
City of Astoria, Community Development Department, 1095 Duane Street, Astoria OR 97103;
or email comments to rjohnson@astoria.or.us.



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Riverfront Vision Plan Implementation Neighborhood Greenway Area Town Hall Meeting 8/20/15

Comment Form

Your comments will be considered in updating the City's development code to implement the Riverfront Vision Plan for properties in the Neighborhood Greenway area.

Not blocking housing views
Putting bathrooms at LaPlante Park
that are auto-locked from 8pm-8am

Extending the Trolley down to
LaPlante Park so Alderbrook
residents can make use of Trolley

No private residential docks
No covered marinas which would
block any homeowners view
change A3 zone to A4 zone

Optional - Name & email: _____

Address: _____

Thank you for your comments! Please leave this completed comment form with one of the project team members before you leave the meeting, or return it to:

City of Astoria, Community Development Department, 1095 Duane Street, Astoria OR 97103;
or email comments to rjohnson@astoria.or.us.

To Shayna 8/24/15

Sherri Williams

From: Rosemary Johnson
Sent: Friday, August 21, 2015 1:21 PM
To: Kit Ketcham
Cc: Kevin Cronin; Sherri Williams; Angela Cosby; rcrater@columbiaestuary.org
Subject: RE: tent caterpillars along the Riverwalk

Thanks for your comments. I am forwarding your question on the caterpillars to our Parks Director to see if she has dealt with this yet. Since it is also along the estuary, I'm forwarding it to CREST to see if they have any input on what to do. We will add your email to the comments from last night. Hope to see you at future work sessions on this issue. Rosemary

Rosemary Johnson, Special Projects Planner
City of Astoria
1095 Duane Street
Astoria OR 97103
503-338-5183
rjohnson@astoria.or.us
www.astoria.or.us

From: Kit Ketcham [<mailto:lilyloosy4@gmail.com>]
Sent: Friday, August 21, 2015 10:47 AM
To: Rosemary Johnson
Subject: tent caterpillars along the Riverwalk

Dear Rosemary (the Wonder Woman of Astoria's Special Projects!),

One compliment and one thing I forgot to bring up at last night's meeting:

You definitely are a wonder! You did a great job last night helping us sort through the many issues potentially affecting Astoria's beloved Alderbrook neighborhood. Thanks for your encyclopedic knowledge of the background and legal matters/ordinances that are part of the situation. You were skilled at repeating people's comments accurately---really important! The meeting was informative and helpful for this fairly new Alderbrook resident.

What I would like to add to the commentary is a question: is there a plan for taking steps to control the tent caterpillars that have begun to infest the alders along the path? I moved to the North Coast from Whidbey Island, which had a huge problem with tent caterpillars several times while I was there. I'm not sure of the best method for dealing with them; they're mostly ugly, I think, but can damage trees irreparably under some circumstances.

Anyhow, just wanted to throw in another two cents worth and offer kudos on how you handled the meeting last night. Thanks.

Kit Ketcham (who now lives at 5360 Alder St., just steps from the Riverwalk entrance.)

--

Kit Ketcham
Minister Emerita, Unitarian Universalist Congregation of Whidbey Island

SRV
Sherri Williams

From: Rosemary Johnson
Sent: Thursday, August 20, 2015 5:10 PM
To: dhweber@gmail.com
Cc: Sherri Williams
Subject: Re: Neighborhood Greenway comments

Will add you to our mailing list and touch base with you later. Thanks

Sent from my iPhone

On Aug 19, 2015, at 10:06 PM, David Weber <dhweber@gmail.com> wrote:

Hi, Rosemary.

This is David Weber at 4623 Ash St. in Alderbrook. I just read the notice on the city website about accepting comments on the Neighborhood Greenway project tomorrow night. Unfortunately, I'm traveling this week and won't be able to attend.

I'm very interested in anything that affects the greenway and have approached the city engineering folks about allowing an extension of the walking path from Violet Laplante park through the greenway and through the back of my property to Birch St. If I can help in any way, please let me know.

Thanks,
David

dhweber@gmail.com
+1.512.377.9090

8/21/15 Email to Shayna, Rd & KC

Sherri Williams

From: Elizabeth Ketcham <kitketcham@me.com>
Sent: Friday, August 21, 2015 12:20 PM
To: Sherri Williams
Subject: alderbrook comments

I am concerned that additional comments pertaining to last night's Alderbrook town hall are to be sent to Rosemary Johnson at this address. However, one gets an immediate message saying that Rosemary is retired and we should contact somebody else.

The handout that we received last night is in error, because it specifically directs additional comments to Ms. Johnson. It seems that this could have been corrected before the erroneous information was put in writing on the handout.

But having found your email address, I will submit this comment: the tent caterpillars are starting to infest the trees along the Riverwalk path. They can defoliate trees quickly if they get a foothold in an area.

Having moved here from Whidbey Island in WA, I am painfully aware of the damage tent caterpillars can do and I urge the city to take protective and preventative measures to limit the damage.

Sincerely,
Elizabeth Ketcham
5360 Alder St.

Sherri Williams

From: Dolores Skillstad <four.doves@live.com>
Sent: Tuesday, September 01, 2015 3:49 PM
To: Sherri Williams
Subject: Removal of Youngs River/Bay from Vision Plan



To: Planning Commission
From: Dolores Skillstad

Dear Commissioners

As discussed with Sherri Williams yesterday, I make this formal request to please remove the reference and enclosure of Youngs River/Bay in the Waterfront Vision Plan since it has never had any prior mention or discussion or approval or acceptance of Youngs River/Bay on the South Slope in the Waterfront Vision Plan.

Thank you.

Dolores A. Skillstad

MEETING SUMMARY

Astoria Neighborhood Greenway Code Amendments Planning Commission Work Session September 1, 2015

City staff and Matt Hastie of Angelo Planning Group conducted a work session with the Astoria Planning Commission on September 1, 2015.

Rosemary Johnson and Matt Hastie gave a brief PowerPoint presentation summarizing a proposed set of code amendment recommendations related to the following topics which are described in more detail in a memo provided to the Planning Commission in advance of the meeting.

- Potential rezoning of the overwater area from the A-3 to the A-4 zone (Aquatic Conservation) with a more limited set of allowed uses in this area.
- Restrictions on the height of new overwater structures to be no higher than the adjacent river bank.
- No changes to the base zones on land within the area.
- Addition of new design standards and/or guidelines for future development, with the exception of single-family and two-family residential structures.
- Landscaping standards which would apply to new development, including areas adjacent to the river.

Rosemary and Matt also noted that the amendments responded to feedback from citizens at the Town Hall meeting with Neighborhood Greenway residents and property owners and other Astoria citizens on August 20 to review and seek feedback on planning issues and Astoria Riverfront Vision goals for the area. After the presentation, community members provided comments, followed by Planning Commission discussion and recommendations. Comments are summarized below.

Public Comments

Comments included the following.

- Alderbrook residents love the area for its quirky nature and scenic beauty. They generally mind their own business, work issues out amongst themselves and do not believe any new regulations are needed. They generally want to be left alone.
- It will be impractical for most overwater uses to occur, including marinas or fishing docks, given tidal conditions in the area. The new requirements seem unnecessary.
- Consider changing the area zoned as Institutional to a Shoreland zone. People visit this area because of its natural resources and peaceful nature. The River Trail offers opportunities for walking, picnicking, swimming, etc. Any changes to the regulations should be intended to keep things the way they are.
- What does the River Trail have to do with development in Alderbrook? What does the City have planned for this area and what is the point of the proposed code changes? The City should not regulate roof types in this area.
- There are some instances where people might want or need large accessory structures (e.g., pole barns to hold recreational vehicles or boats). Lots of people here don't have issues with those types of structures and if I or my neighbor want to build one, that should be OK.

In responding to public comments, Rosemary and Matt noted the following:

- The City does not have any development plans for the area. The code amendments would address potential future development proposals submitted by property owners or developers. There are no current proposals but they are a possibility in the future.
- The point of the code amendments is to implement recommendations from the Riverfront Vision Plan for this part of Astoria. There was strong participation in the Vision Plan by Alderbrook and other City residents. The general intent of the recommendations is to retain the current character and conditions in Alderbrook, consistent with comments provided at this meeting. Current code provisions allow types of development that could change the character of the area. The point of most of the proposed code amendments is to retain the area's character.
- The design standards or guidelines proposed for the area would be applied to a relatively limited set of uses – commercial, institutional or multi-family structures – which represent larger scale uses that could affect the character of the area. They would not be applied to single-family or two-family residential homes.
- Potential requirements associated with accessory structures (e.g., pole barns, garages, etc.) are intended to address land use issues previously raised by area residents, particularly the height, scale, and design of such structures.
- The City's Institutional zone allows for community uses such as schools, parks or similar uses that would otherwise be allowed only as conditional uses in a given area. This zone also includes specific standards for such uses. Changing the existing Institutional zone to a Shoreland zone could cause issues for future park-related structures in this area in terms of their design and permitting (e.g., restrooms, pedestrian bridges, etc.).

Planning Commission Discussion and Direction

Planning Commissioners provided the following comments:

- Commissioners generally voiced support for most of the recommendations and concepts presented by staff.
- The City should pursue changing the overwater zoning from A3 to A4. A majority of people who attended and spoke at the Town Hall meeting supported that proposal.
- No size restrictions are recommended for accessory structures; similarly design review should not be required for those structures.
- Guidelines and standards are appropriate for multi-family residential development (where State laws requires clear and objectives standards but where guidelines may be used as an alternative path). Other types of uses (e.g., commercial or institutional) should just use design guidelines to provide more flexibility.
- Design guidelines are important for the types of uses proposed by staff in that they will allow residents to speak to the character of specific development proposals.
- Some standards for roof types should be considered.

Staff will incorporate direction from the Planning Commission and public into a draft set of draft code amendments for review and discussion at a subsequent Planning Commission work session and hearing. The next work session is scheduled for September 16 and the hearing is scheduled for October 27.



November 10, 2015

TO: MAYOR AND CITY COUNCIL

FROM:  BRETT ESTES, CITY MANAGER

SUBJECT: APPEALS BY RON ZILLI FOR THE WIRELESS COMMUNICATION FACILITY PERMITS AT 1580 SHIVELY PARK ROAD
APPEAL (AP15-01) ON NEW CONSTRUCTION PERMIT (NC15-03)
APPEAL (AP15-02) ON VARIANCE (V15-03)
APPEAL (AP15-03) ON WIRELESS COMMUNICATION FACILITY (WCF15-03)

BACKGROUND

On August 3, 2015, Verizon Wireless LLC applied for a New Construction permit (NC15-03) to the Historic Landmarks Commission (HLC) to construct a new wireless communication facility at 1580 Shively Park Road within Shively Park. On September 15, 2015, the HLC held a public hearing and approved the request with conditions.

On August 3, 2015, Verizon Wireless LLC applied for a Wireless Communications Facility permit (WCF15-03) and a Variance (V15-03) to the Astoria Planning Commission (APC) to construct a new wireless communication facility with a height of 150' at 1580 Shively Park Road within Shively Park. On September 16, 2015, the APC held public hearings and approved the requests with conditions.

A Notice of Appeal on the HLC and APC decisions was submitted by Ron Zilli on September 30, 2015. A public hearing on the Appeals has been advertised and is scheduled for the November 16, 2015 City Council meeting.

The applicant, Verizon Wireless, has requested that the hearing be continued to the December 21, 2015 City Council meeting. The applicant has submitted the necessary documents for the continuances and the appellant has been notified. Staff agrees that the request for a continuance is appropriate. It would be in order for the Council to continue each of the public hearings to the December 21, 2015 City Council meeting.

RECOMMENDATION

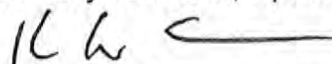
It is recommended that the City Council continue the public hearing on the Appeals (Ap15-01, AP15-02, AP15-03) to the December 21, 2015 City Council meeting at 7:00 pm.

By:



Rosemary Johnson, Special Projects Planner

Through:



Kevin Cronin, Community Development Director



CITY OF ASTORIA
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November 5, 2015

MEMORANDUM

TO: MAYOR AND CITY COUNCIL

FROM:  BRETT ESTES, CITY MANAGER

SUBJECT: **RIGHT TURN PERMITTED WITHOUT STOPPING INVESTIGATION**

DISCUSSION/ANALYSIS

There are several primary routes through the City that allow vehicular traffic less restricted mobility. These routes have historically utilized "Right Turn Permitted Without Stopping" (RTPWS) secondary signs below stop signs to facilitate this movement. While this existing condition has proved successful for many years, it does have some drawbacks that have resulted in complaints. Recently, the City received several complaints specific to the RTPWS signs.

DKS Associates investigated the situation to provide recommendations on how to maintain a similar level of mobility, while addressing the challenges. DKS is a transportation engineering and planning firm that has extensive experience working in Astoria. They recently completed both the City of Astoria and Clatsop County Transportation System Plans. These were multi-year efforts that required detailed knowledge of the City's transportation system and the regional transportation network.


DKS worked closely with City Staff to review the relevant travel paths through town, identify the associated challenges, and provide recommendations for improvement. This work is summarized in the attached technical memo and will be presented to City Council at the November 16th meeting. Staff and DKS presented the results of their study to the Traffic Safety Advisory Committee at their October 27th meeting. The Traffic Safety Committee agreed with the recommended Option 4 and passed a motion to recommend that Option 4 be implemented.

As discussed with the Traffic Safety Committee, if Option 4 is implemented, Staff will prepare detailed exhibits of the improvements prior to implementation and add in any other relevant improvements to enhance the configuration of the intersections. Examples include the possible placement of a dedicated right turn lane at the foot of 7th Street turning northbound onto Hwy 202 and "Stop Sign Ahead" signs where stop sign violations are known to occur at a rate higher than expected.

Amending traffic flow would not normally require City Council concurrence; as such changes would need to comply with Manual of Uniform Traffic Devices (MUTD) standards. That being said, these changes will have costs to be borne by the City. It is estimated that the cost for these changes will be around \$10,000. This issue is being forwarded to Council to determine if funds should be allocated to address the concerns. Should Council concur with Option 4, Public Works staff will work to implement within the current fiscal year budget.

RECOMMENDATION

It is requested that City Council consider the recommendations made DKS and the City of Astoria Traffic Safety Committee to move forward with Option 4.

Submitted By 
Ken P. Cook, Public Works Director

Prepared By JEFF HARRINGTON
Jeff Harrington, City Engineer



720 SW Washington St.
 Suite 500
 Portland, OR 97205
 503.243.3500
 www.dksassociates.com

MEMORANDUM

DATE: October 26, 2015
 TO: Jeff Harrington, City of Astoria
 FROM: Chris Maciejewski, P.E., P.T.O.E., DKS Associates
 Kevin Chewuk, P.T.P., DKS Associates

SUBJECT: Astoria Traffic Control Analysis P15069-000

This memorandum summarizes an analysis performed to determine the motor vehicle impacts associated with potential traffic control changes at ten intersections in the city of Astoria (see Figure 1). The expected advantages and impacts of different traffic control options were assessed.

EXISTING CONDITIONS

Most of the study intersections have an approach controlled with a stop sign, with a “RIGHT TURN PERMITTED WITHOUT STOPPING” sign displayed below it. According to the Oregon Driver Manual, this sign allows traffic turning right to turn without stopping. Other traffic must stop and yield right of way. The city of Astoria has received feedback from residents that these intersections can be confusing, primarily from visitors to the city who may not be familiar with the intersections. This issue was not brought up during the recent Transportation System Plan update, so improvements were not recommended at that time. The following provides a summary of the existing context of these intersections.



Figure 1: Study Intersections

1. Niagara Avenue/ 7th Street

This intersection has four approaches, with the primary movement between the south leg (7th Street) and east leg (Niagara Avenue) of the intersection (see Figure 2). To facilitate the primary movements, the westbound Niagara Avenue approach has no intersection control (free movement) and the northbound 7th Street approach includes a stop sign, with a “RIGHT TURN PERMITTED WITHOUT STOPPING” sign. The eastbound, and southbound approaches to the intersection are controlled with stop signs.

2. Niagara Avenue/ 8th Street

This intersection has four approaches, with the primary movement between the west leg (Niagara Avenue) and north leg (8th Street) of the intersection (see Figure 2). To facilitate the primary movements, the eastbound Niagara Avenue approach has no intersection control (free movement) and the southbound 8th Street approach includes a stop sign, with a “RIGHT TURN PERMITTED WITHOUT STOPPING” sign. The westbound, and northbound approaches to the intersection are controlled with stop signs.



Figure 2: Context of the Niagara Avenue/7th Street, and Niagara Avenue/8th Street intersections

3. Niagara Avenue/ 15th Street

This intersection has three approaches, with the primary movement between the west leg (Niagara Avenue) and north leg (15th Street) of the intersection (see Figure 3). To facilitate the primary movements, the eastbound Niagara Avenue approach has no intersection control (free movement) and the southbound 15th Street approach includes a stop sign, with a “RIGHT TURN PERMITTED WITHOUT STOPPING” sign. The westbound approach to the intersection is controlled with a stop sign.



Figure 3: Context of the Niagara Avenue/15th Street intersection

4. Jerome Avenue/ 14th Street

This intersection has four approaches, with the primary movement between the north leg (14th Street) and east leg (Jerome Avenue) of the intersection (see Figure 4). To facilitate the primary movements, the southbound 14th Street approach has no intersection control (free movement) and the westbound Jerome Avenue approach includes a stop sign, with a “RIGHT TURN PERMITTED WITHOUT STOPPING” sign. The eastbound, and northbound approaches to the intersection are controlled with stop signs.

5. Jerome Avenue/ 15th Street

This intersection has three approaches, with the primary movement between the east leg (Jerome Avenue) and south leg (15th Street) of the intersection (see Figure 4). To facilitate the primary movements, the westbound Jerome



Figure 4: Context of the Jerome Avenue/14th Street, Jerome Avenue/15th Street, and Jerome Avenue/16th Street intersections

Avenue approach has no intersection control (free movement) and the northbound 15th Street approach includes a stop sign, with a “RIGHT TURN PERMITTED WITHOUT STOPPING” sign. The eastbound approach to the intersection is controlled with a stop sign.

6. Jerome Avenue/ 16th Street

This intersection has four approaches, with the primary movement between the west leg (Jerome Avenue) and north leg (16th Street) of the intersection (see Figure 4). The southbound 16th Street approach has no intersection control (free movement). The northbound, eastbound, and westbound approaches to the intersection are controlled with stop signs. The southbound 16th Street approach to the intersection is a free movement.

7. Irving Avenue/ 8th Street

This intersection has four approaches, with the primary movement between the east leg (Irving Avenue) and south leg (8th Street) of the intersection (see Figure 5). To facilitate the primary movements, the westbound Irving Avenue approach has no intersection control (free movement). The northbound 8th Street approach has no control (free movement) for the right-turn movement, and a yield for the through and left-turn movements.



Figure 5: Context of the Irving Avenue/8th Street intersection

8. Irving Avenue/ 11th Street

This intersection has three approaches, with the primary movement between the west leg (Irving Avenue) and north leg (11th Street) of the intersection (see Figure 6). To facilitate the primary movements, the eastbound Irving Avenue approach has no intersection control (free movement) and the southbound 11th Street approach includes a stop sign, with a “RIGHT TURN PERMITTED WITHOUT STOPPING” sign. The westbound approach to the intersection is controlled with a stop sign.



Figure 6: Context of the Irving Avenue/11th Street intersection

9. Harrison Avenue/ 33rd Street

This intersection has four approaches, with the primary movement between the north leg (33rd Street) and east leg (Harrison Avenue) of the intersection (see Figure 7). To facilitate the primary movements, the southbound 33rd Street approach has no intersection control (free movement) and the westbound Harrison Avenue approach includes a stop sign, with a “RIGHT TURN PERMITTED WITHOUT STOPPING” sign. The northbound, and eastbound approaches to the intersection are controlled with stop signs.



Figure 7: Context of the Harrison Avenue/33rd Street intersection

10. Highway 202/ 7th Street

This intersection has three approaches, with the primary movement between the west leg (Highway 202) and north leg (7th Street) of the intersection (see Figure 8). To facilitate the primary movements, the eastbound Highway 202 approach has no intersection control (free movement) and the southbound 7th Street approach includes a stop sign, with a “RIGHT TURN PERMITTED WITHOUT STOPPING” sign. The westbound approach to the intersection is controlled with a stop sign. This intersection is under the jurisdiction of ODOT.



Figure 8: Context of the Highway 202/7th Street intersection



Collision Evaluation

Collision data from the most recent five years of available data (2009 to 2014¹) for all study intersections was obtained from ODOT and reviewed. The total number of crashes experienced at an intersection is typically proportional to the number of vehicles entering it. Therefore, a crash rate describing the frequency of crashes per million entering vehicles (MEV) is used to determine if the number of crashes should be considered high. Using this technique, a collision rate of 1.0 MEV or greater is commonly used to identify when collision occurrences are higher than average and should be further evaluated. The crash rates calculated for the study intersections can be seen in Table 1. None of the observed crash rates at the study intersections are above the 1.0 million entering vehicles (MEV) threshold, indicating the frequency of collisions is normal for the volume of traffic served.

Table 1. Study Intersection Collision Analysis (2009-2014)

Study Intersection	Crash Severity				Crash Total	Observed Crash Rate (MEV)
	Fatal	Major or Moderate Injury	Minor Injury	Property Damage Only		
1 Niagara Avenue/ 7th Street	0	1	1	2	4	0.33
2 Niagara Avenue/ 8th Street	0	2	0	3	5	0.46
3 Niagara Avenue/ 15th Street	0	0	0	2	2	0.25
4 Jerome Avenue/ 14th Street	0	0	0	0	0	0.00
5 Jerome Avenue/ 15th Street	0	0	0	1	1	0.12
6 Jerome Avenue/ 16th Street	0	1	2	0	3	0.31
7 Irving Avenue/ 8th Street	0	0	1	0	1	0.10
8 Irving Avenue/ 11th Street	0	0	0	0	0	0.00
9 Harrison Avenue/ 33rd Street	0	0	0	2	2	0.71
10 Highway 202/ 7th Street	0	0	0	4	4	0.27

Traffic Control Options

Four options for traffic control were analyzed at the study intersections. Some of these options would remove the “Right-Turn Permitted without Stopping” signs at the study intersections, while others would improve signage and roadway striping. These options are summarized below.

¹ ODOT reported collisions for August 31, 2009 to August 31, 2014.



Option 1: Stop Control

The “Right-Turn Permitted without Stopping” signs would be removed at all study intersections. For the purposes of this analysis, these movements were assumed to become stop controlled (i.e., no additional traffic control changes were assumed at the intersections). The removal of the signage would reduce the driver confusion factor that sometimes occurs at these intersections, however, this option would result in increased delay for drivers at these approaches. A sketch level analysis was reviewed at these intersections to estimate the additional delay drivers may experience, based on collected p.m. peak traffic volume data from the Transportation System Plan or estimated p.m. peak traffic volume data derived from the travel demand model. The results are summarized below.

1. **Niagara Avenue/ 7th Street:** The northbound right-turn from 7th Street to Niagara Avenue was assumed to be modified from a free movement to stop control. This change would add an additional 10 seconds of delay per driver making the turn, or approximately 48 minutes of total delay during the p.m. peak hour.
2. **Niagara Avenue/ 8th Street:** The southbound right-turn from 8th Street to Niagara Avenue was assumed to be modified from a free movement to stop control. This change would add an additional 10.5 seconds of delay per driver, or approximately 32 minutes of total delay during the p.m. peak hour.
3. **Niagara Avenue/ 15th Street:** The southbound right-turn from 15th Street to Niagara Avenue was assumed to be modified from a free movement to stop control. This change would add an additional 11 seconds of delay per driver, or approximately 25 minutes of total delay during the p.m. peak hour.
4. **Jerome Avenue/ 14th Street:** The westbound right-turn from Jerome Avenue to 14th Street was assumed to be modified from a free movement to stop control. This change would add an additional 9 seconds of delay per driver, or approximately 4 minutes of total delay during the p.m. peak hour.
5. **Jerome Avenue/ 15th Street:** The northbound right-turn from 15th Street to Jerome Avenue was assumed to be modified from a free movement to stop control. This change would add an additional 10 seconds of delay per driver, or approximately 31 minutes of total delay during the p.m. peak hour.
6. **Jerome Avenue/ 16th Street:** No changes were assumed at this intersection under Option 1. Delay per driver would remain unchanged.
7. **Irving Avenue/ 8th Street:** The northbound right-turn from 8th Street to Irving Avenue was assumed to be modified from a free movement to stop control. This change would add an additional 12 seconds of delay per driver, or approximately 27 minutes of total delay during the p.m. peak hour.
8. **Irving Avenue/ 11th Street:** The southbound right-turn from 11th Street to Irving Avenue was assumed to be modified from a free movement to stop control. This change would add an additional 10 seconds of delay per driver, or approximately 28 minutes of total delay during the p.m. peak hour.
9. **Harrison Avenue/ 33rd Street:** The westbound right-turn from Harrison Avenue to 33rd Street was assumed to be modified from a free movement to stop control. This change would add an additional 9 seconds of delay per driver, or approximately 9 minutes of total delay during the p.m. peak hour.
10. **Highway 202/ 7th Street:** The southbound right-turn from 7th Street to Highway 202 was assumed to be modified from a free movement to stop control. This change would add an additional 12 seconds of delay per driver, or approximately 42 minutes of total delay during the p.m. peak hour.

Overall, drivers traveling between the south and north ends of the city would be expected to experience a travel time increase between 30 seconds and one minute. This would represent a 20 percent increase in travel time. Although



drivers would be expected to experience increased delay at these intersections, they likely will not divert to alternate routes due to a higher number of stops and limited connectivity along adjacent streets.

During field reviews, it was noted that adding stop control to some of these movements could create an issue with stopping on steep hills when icy. In addition, a cursory field review found no apparent issues with sight distance at the study intersections, with the exception of the Highway 202/7th Street intersection. At this intersection, a vertical curve on the westbound Highway 202 approach limits sight distance. However, that approach would not be impacted under Option 1.

Option 2: All-Way Stop Control

All study intersections would be controlled with all-way stops (with the exception of the Highway 202/ 7th Street intersection; no changes would occur at this intersection under Option 2). The “Right-Turn Permitted without Stopping” signs, and any free movements would be replaced with stop signs.

Option 2 has a fatal flaw in that the study intersections do not meet the Manual on Uniform Traffic Control Devices (MUTCD) warrant for all-way stop control. The warrant requires five or more reported crashes in a 12-month period; none of the study intersections had more than three crashes in a single year. In addition, the intersections do not meet the minimum entering volume criteria.

The study intersections are located along routes that are meant to provide local travel between the north and south end of the city. If unwarranted all-way stop control is installed, studies have shown that driver compliance will likely be poor and safety could be decreased. Drivers may feel that the signs have no traffic control purpose, and that there is little reason to yield the right-of-way because there are usually no vehicles on the minor street. Studies have also shown that the safety of pedestrians is decreased with unwarranted all-way stop control. Pedestrians expect vehicles to stop at the stop signs, but many drivers get in the habit of running the unwarranted stop sign.

The Astoria Fire Chief is strongly opposed to any changes to vehicle traffic patterns along primary response routes, including through the Highway 202/7th Street intersection. Changing traffic patterns could have a negative impact on their response time.

Option 3: Free Movements with the Through Route of the Intersection Delineated

The “Right-Turn Permitted without Stopping” signs would be removed at all study intersections. All movements that were previously uncontrolled, or controlled with the “Right-Turn Permitted without Stopping” signs would be free movements (with the exception of the Jerome Avenue/14th Street intersection, as detailed below). To facilitate this, the roadway striping would be continued through the intersection to delineate the through movement of traffic. The removal of the signage would reduce the driver confusion factor that sometimes occurs at these intersections, without causing an increase in delay for drivers that would be experienced under Option 1 or 2.

Due to the low through traffic demand at the Jerome Avenue/ 14th Street intersection, this free-movement concept was not assumed. Instead, the intersection was assumed with stop control on the Jerome Avenue approaches, and free movements on the 14th Street approaches. The potential Option 3 concept is illustrated at the study intersections in Figures 9 through 15.



Figure 9: Potential Configuration of Niagara Avenue/7th Street-8th Street Intersections under Option 3



Figure 10: Potential Configuration of Niagara Avenue/15th Street Intersection under Option 3



Figure 11: Potential Configuration of Jerome Avenue/14th Street-15th Street-16th Street Intersections under Option 3

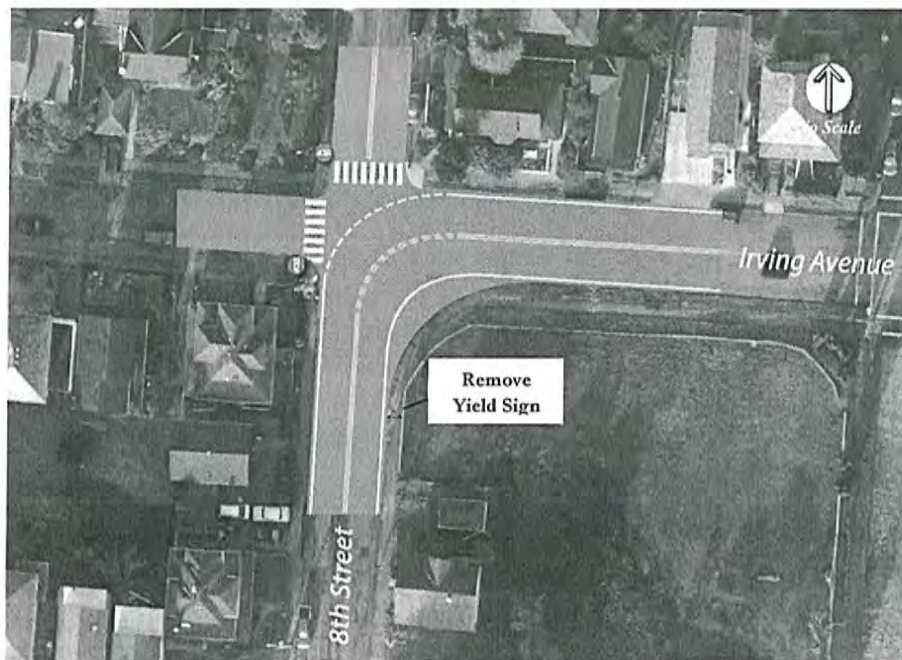


Figure 12: Potential Configuration of Irving Avenue/8th Street Intersection under Option 3



Figure 13: Potential Configuration of Irving Avenue/ 11th Street Intersection under Option 3



Figure 14: Potential Configuration of Harrison Avenue/33rd Street Intersection under Option 3



Figure 15: Potential Configuration of Highway 202/7th Street Intersection under Option 3

Option 3 has a fatal flaw in that it is opposed by the Astoria Police Chief, and could potential create driver confusion on who has right-of-way at an intersection. The Oregon Drivers Manual states that at an approach where there is no traffic control, you must look and yield the right of way to any vehicle in the intersection or approaching from your right at the same time. Under Option 3, the uncontrolled movements at study intersections would be left and right-turns, which is not a typical intersection configuration and could be confusing to some drivers.

Option 4: Roadway Striping, Signage, and Supplemental Intersection Improvements

All city study intersections would include improved roadway striping to delineate the through movement of traffic (no roadway striping changes would occur at the Highway 202/7th Street intersection). The “Right-Turn Permitted without Stopping” signs would be replaced with the approved MUTCD sign “Except Right Turn.” Additional supplemental signage improvements would be incorporated as appropriate, including intersection warning signs. All movements that were previously uncontrolled, or controlled with the “Right-Turn Permitted without Stopping” signs would be free movements. No changes in vehicle delay or stopping would be expected under Option 4. This option is illustrated in the following sections.

Irving Avenue/ 8th Street

This intersection has four approaches, with the primary movement between the east leg (Irving Avenue) and south leg (8th Street) of the intersection. To facilitate the primary movements, the westbound Irving Avenue approach has no intersection control (free movement). The northbound 8th Street approach has no control (free movement) for the right-turn movement, and a yield for the through and left-turn movements.

A potential modification to this intersection could be to modify the northbound 8th Street approach to stop control for the through and left-turn movements, and yield control for the right-turn movement (see Figure 16). The stop bar for this approach should be aligned with the curb of the eastbound Irving Avenue approach, with a striped island helping to better delineate the travel routes.

The stop signs at the northbound, southbound, and eastbound approaches should be supplemented with signage noting that traffic from westbound Irving Avenue is not required to stop. Advanced stop control warning signs could be added as a supplement to the 8th Street approaches.



Figure 16: Potential Configuration of Irving Avenue/ 8th Street Intersection

Other Study Intersections

A potential modification could be to improve the striping of the streets to better delineate the travel routes. The “Right-Turn Permitted without Stopping” signs should be updated to include the MUTCD sign “Except Right Turn.” The concept is illustrated in Figures 17 to 21.



Figure 17: Potential Configuration of Niagara Avenue/15th Street Intersection under Option 4

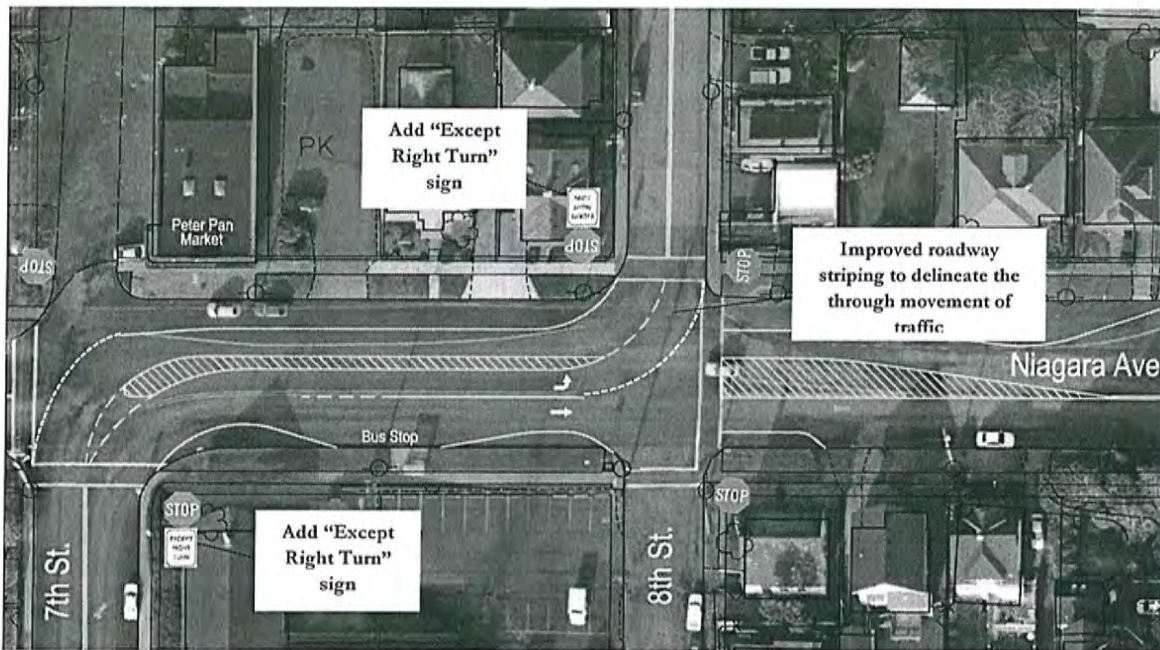


Figure 18: Potential Configuration of Niagara Avenue/ 7th Street and Niagara Avenue/ 8th Street Intersections under Option 4



Figure 19: Potential Configuration of Jerome Avenue/14th Street-15th Street Intersections under Option 4



Figure 20: Potential Configuration of Irving Avenue/ 11th Street Intersection under Option 4

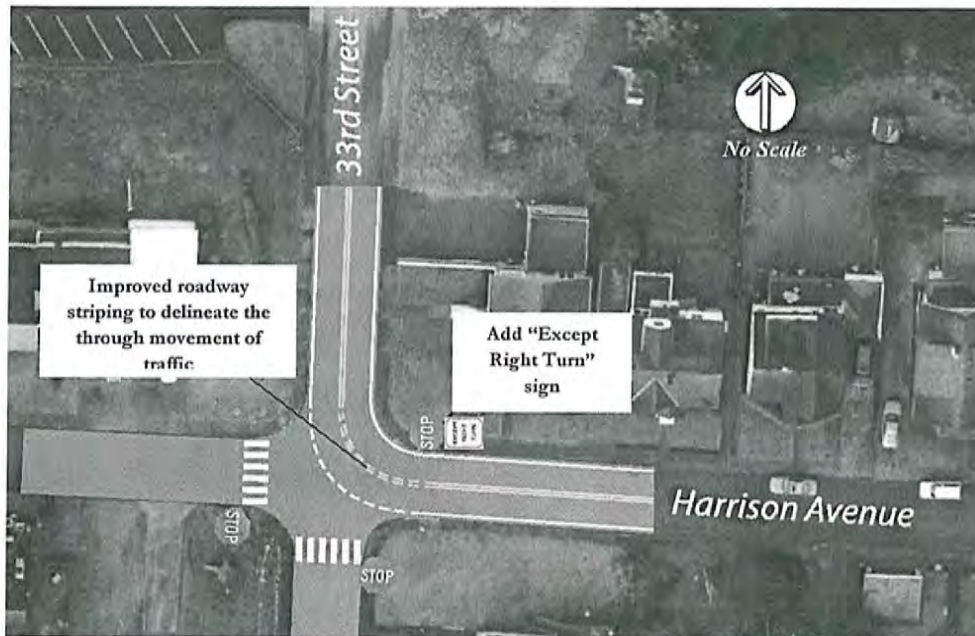


Figure 21: Potential Configuration of Harrison Avenue/33rd Street Intersection under Option 4

RECOMMENDATION

Overall, it is recommended that Option 4 be implemented at all study intersections. These improvements are cost effective, and would retain the free movements, and more clearly delineate the desired through route to match the intent of the streets and the current traffic demand.


The TSP recommended improved intersection control and guidance at the Niagara Avenue/7th Street and Niagara Avenue/8th Street intersections (including the consideration of mini-roundabouts) and other improvements along Niagara Avenue. This option would represent a cost effective interim improvement if the city desires to pursue the TSP improvements in the future.



CITY OF ASTORIA
POLICE DEPARTMENT

November 16, 2015

MEMORANDUM

TO: MAYOR AND CITY COUNCIL
FROM:  BRETT ESTES, CITY MANAGER
SUBJECT: TEMPORARY PUBLIC RESTROOM FACILITIES

DISCUSSION/ANALYSIS

Over the previous summer, the number of complaints the Astoria Police Department has responded to regarding public urination and defecation is greatly increased. We have also heard from business owners in the downtown that this is a major issue affecting them regularly. The clear message received from the Astoria Downtown Historic District Association was that this is not just an issue with the members of our community who are homeless but also for tourists who are visiting the community. While public restrooms are available at the Sunset Empire Transportation District office and the City owned restrooms are available east of 12th on Exchange these facilities are not located where they positively affect this issue.

Since September, the City of Astoria Coalition on Community Homeless Interaction has been meeting. While they are moving toward some other recommendations, one recommendation that was quite unified and was deemed as "low hanging fruit" is to site more public restrooms. This recommendation was unanimous.

Overlaying this issue is the ongoing problems Astoria Parks and Recreation faces with vandalism to existing bathrooms. There has been significant repeated damage to the downtown bathrooms, the Doughboy Monument bathrooms and the bathrooms at Tapiola Park.

Long term, staff has identified a potential permanent solution to the issue of public restrooms with a facility referred to as the Portland Loo. While these units have a fairly high initial purchase price they are designed in a way that incorporates the concepts of crime prevention through environmental design (CEPTED), a proven community policing and planning concept for crime reduction. These solutions would be brought back to Council as part of the budget process for FY 16-17 if they develop.

In the interim, Angela Cosby and Brad Johnston have developed two locations where temporary portable toilet facilities could be located. The criteria for locations were that they had to be:

- High traffic areas that offered easy visual surveillance (the presence of many eyes reduces crime).
- Not over or under combustible materials (to avoid associated fires where portable are vandalized with fire).
- In an area where they can be easily serviced.
- In a location where their presence would not be visually shocking.
- In an area where calls, complaints, and anecdotal evidence demonstrate a need for public restrooms.

Given these criteria the Parks Director, Angela Cosby, and Police Chief, Brad Johnston, recommend that the City contract to place two portable toilets at People's Park (16th and Marine) in the parking area and one portable toilet at 9th and Astor in the right of way near the street terminus where concrete blocks currently block the street. These toilets would be serviced twice a week. Funding for the service and toilets would come from the Promote Astoria Fund as these are services provided to support tourism. Cost for this service is estimated to be \$130.40 per unit per 28 day billing period. Staff recommends purchasing the \$7.95 per billing period damage waiver. If delivery is scheduled to coincide with routine trips, there will be no charge for delivery or set up. The total cost for one year of service is \$5,395.65.

Staff will continue to monitor the service levels and explore the possibility of more permanent solutions that may alleviate some of the issues which cause ongoing problems for Astoria Parks and Recreation with the existing public restrooms.

RECOMMENDATION

Staff recommends contracting for placement of three temporary toilets. Staff will continue to monitor the temporary solution to determine if there is a change in behavior and whether more permanent solutions are appropriate.



Brad Johnston
Chief of Police
Assistant City Manager